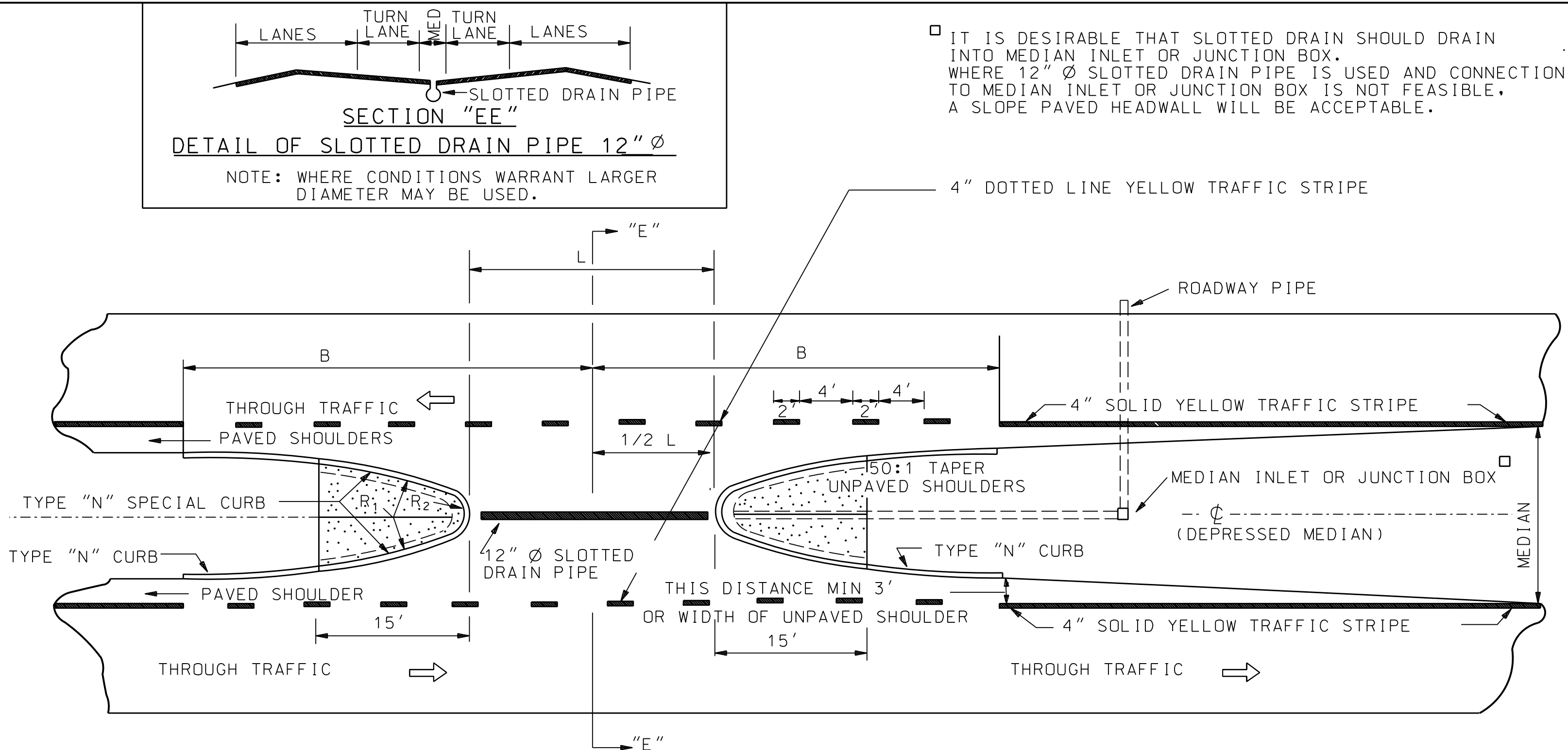
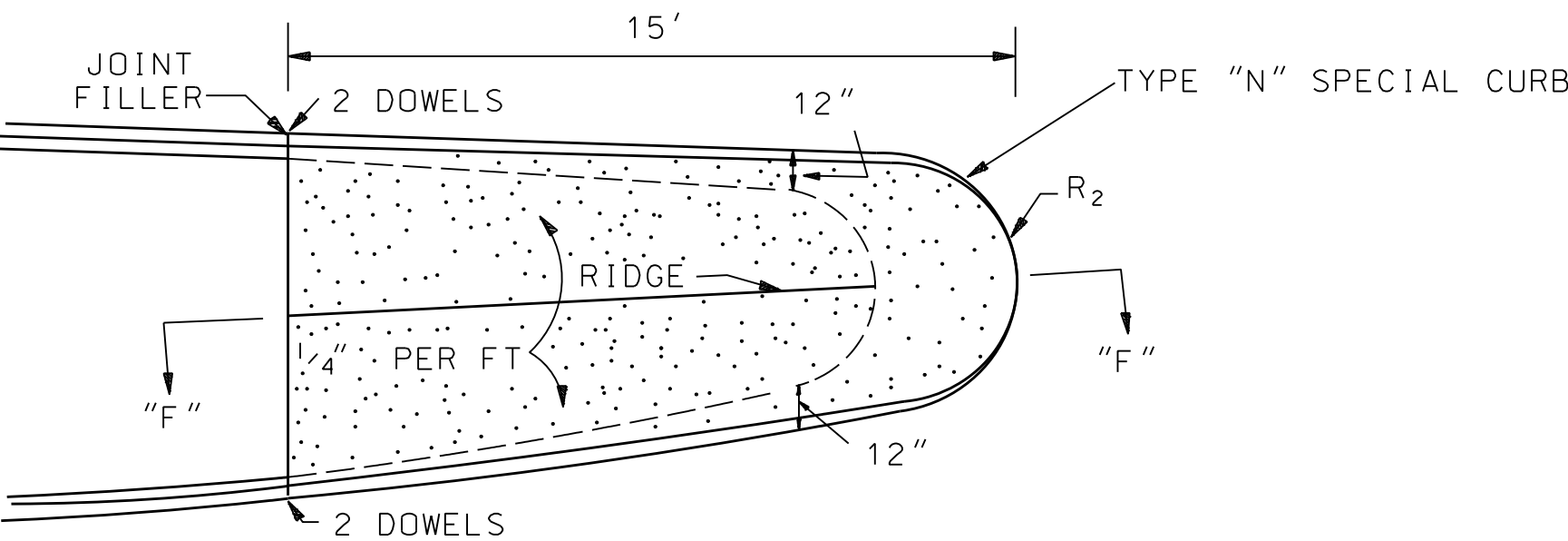


IT IS DESIRABLE THAT SLOTTED DRAIN SHOULD DRAIN INTO MEDIAN INLET OR JUNCTION BOX. WHERE 12" Ø SLOTTED DRAIN PIPE IS USED AND CONNECTION TO MEDIAN INLET OR JUNCTION BOX IS NOT FEASIBLE, A SLOPE PAVED HEADWALL WILL BE ACCEPTABLE.



DETAILS WITH PAVED SHOULDERS ABOVE DETAILS PAVING DETAILS FOR UNPAVED SHOULDERS

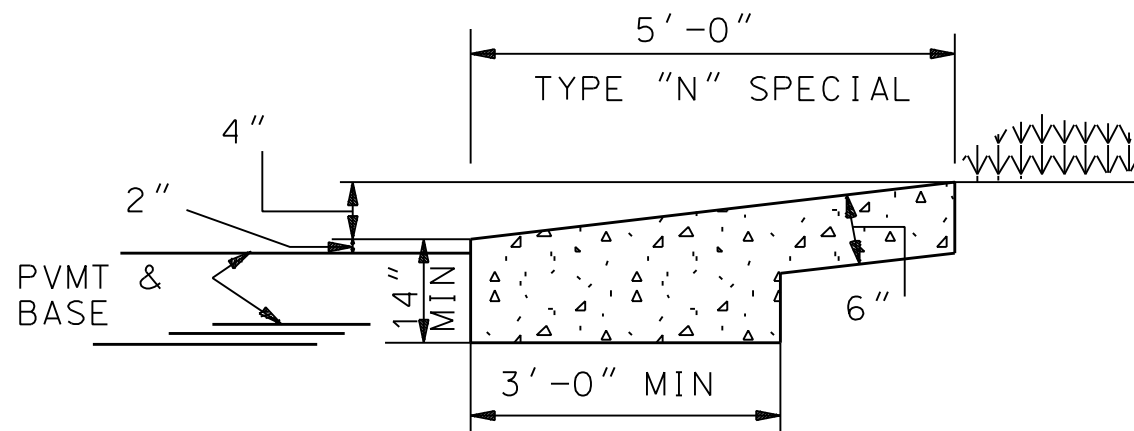
THE 5" OR 6" TRAFFIC STRIPE IS OPTIONAL AT LOCATIONS SHOWN AS 4" STRIPE. THE PAYMENT OF THE 5" OR 6" TRAFFIC STRIPE WILL BE IDENTIFIED IN THE PAY ITEM USED.



TOP VIEW OF GORE PAVING

M WIDTH OF MEDIAN (FEET)	DIMENSIONS IN FEET, WHEN					
	R <sub>1</sub> = 90'		R <sub>1</sub> = 150'		R <sub>1</sub> = 230'	
	L	B	L	B	L	B
20'	58	65	66	78	71	90
30'	48	68	57	85	63	101
40'	40	71	50	90	57	109
50'	-	-	44	95	51	115
60'	-	-	-	-	46	122
70'	-	-	-	-	41	128

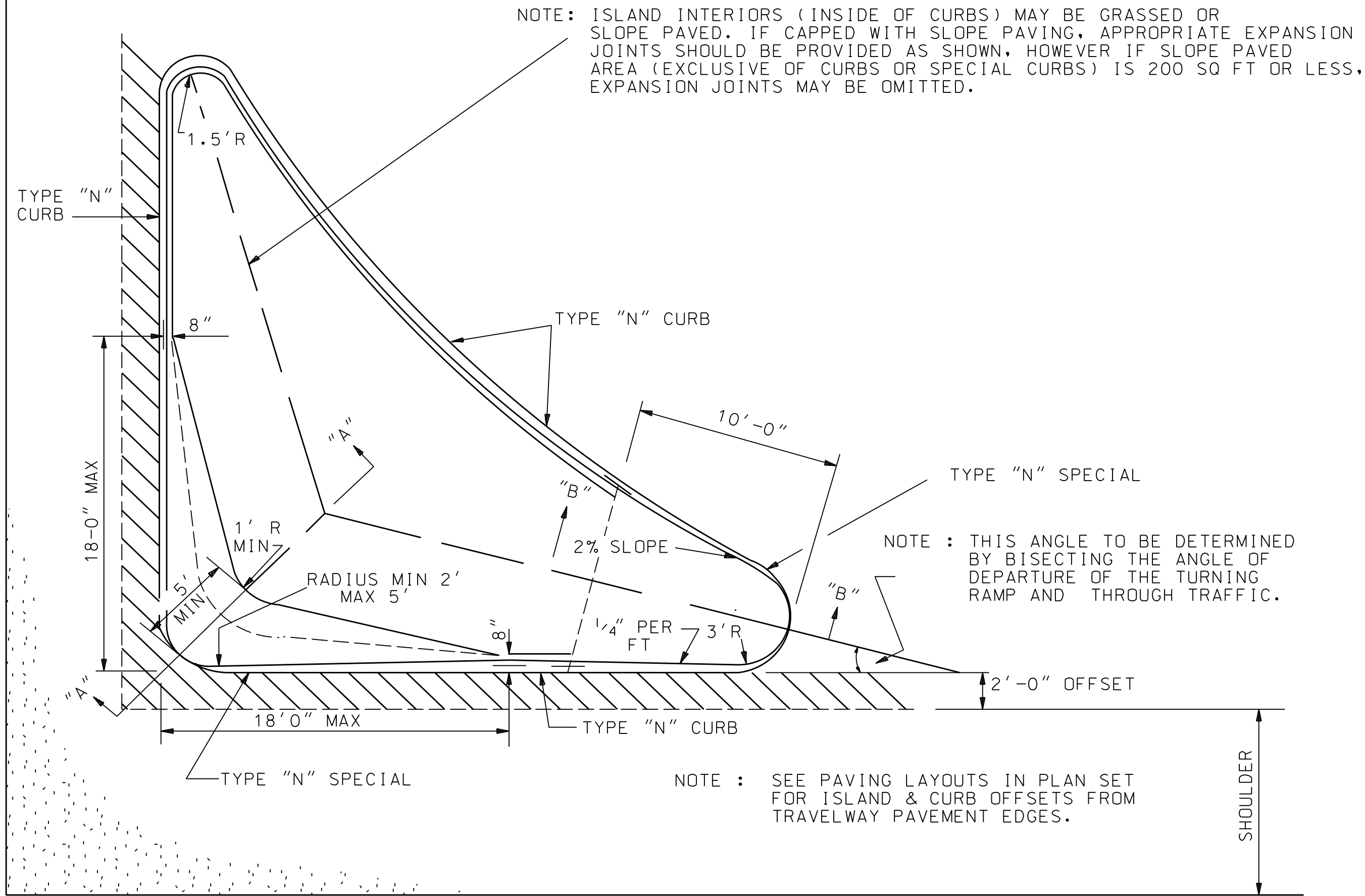
R<sub>2</sub> = 1/2 MEDIAN WIDTH MAX BUT IT IS PLEASING IN PROPORTION AND APPEARANCE WHEN IT IS ABOUT 1/5 OF THE MEDIAN WIDTH.



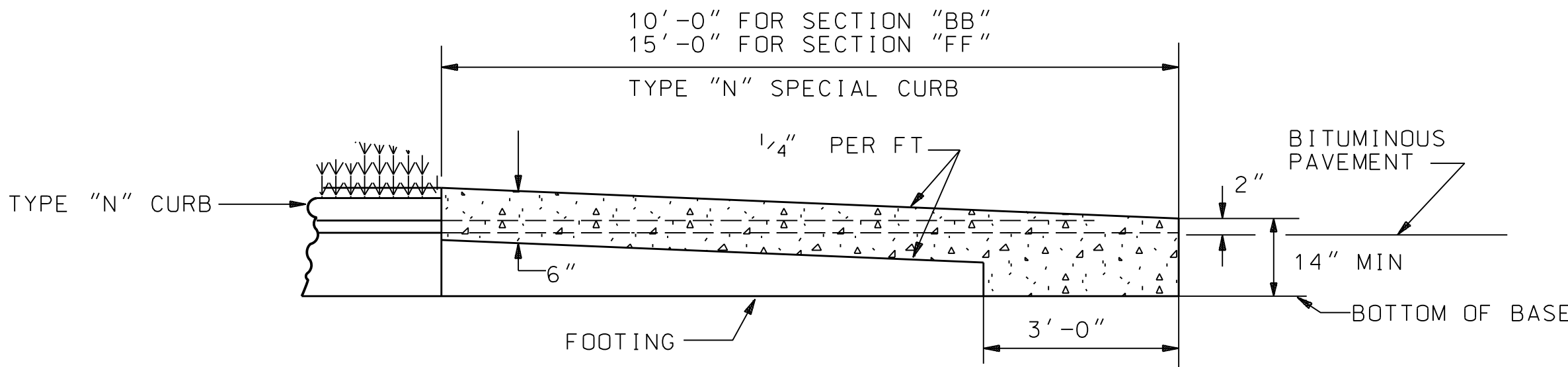
SECTION AA

GENERAL NOTES

- PAYMENT AND MEASUREMENT OF CONC NOSE WILL BE MADE UNDER TYPE "N" (SPECIAL) PER LIN FT. THE GORE SECTION OF TYPE "N" (SPECIAL) SHALL BE MEASURED FROM THE END OF TYPE "N" STANDARD CURB AT THE JOINT AROUND THE GORE IN THE GUTTER TO THE OTHER JOINT.
- WHEN THE ISLAND INTERIOR IS PAVED WITH SLOPE PAVING, THE JOINT WITH THE CURBS SHALL BE A BUTT TYPE CONSTRUCTION JOINT UNLESS OTHERWISE NOTED OR DIRECTED BY THE ENGINEER.
- FILLER AND SEALER FOR EXPANSION JOINTS SHALL BE AS OUTLINED IN ARTICLES 832.01 AND 832.02 ALDOT SPECIFICATIONS.
- FOR HIGH SPEED RURAL DESIGNS NO CURB SHALL BE USED. THE GORE AND NOSE SHALL CONFORM TO SAME GEOMETRIC DESIGN EXCEPT THAT INSIDE SHOULDER PAVEMENT SHALL BE EXTENDED AROUND EDGES OF CROSSOVER. IN THIS CASE THE DRAINAGE PROBLEM WILL BE ELIMINATED AND WILL NOT REQUIRE SLOTTED DRAINS.
- A CORRUGATED SLOTTED DRAIN PIPE WILL BE REQUIRED WHERE PONDING MAY TAKE PLACE IN A VERTICAL SAG OR FLAT GRADE.
- AT MEDIAN CROSSOVERS THE PLACING OF SOLID EDGELINE IS TO BE STOPPED FROM RADIUS POINT TO RADIUS POINT. IN THE SAME BASIC METHOD USED AT CROSS STREET INTERSECTIONS A DOTTED EDGELINE SHALL BE EXTENDED ACROSS THE MEDIAN OPENING FROM RADIUS POINT TO RADIUS POINT IN A PATTERN OF 2 FT LINE LENGTHS AND 4 FT GAPS.

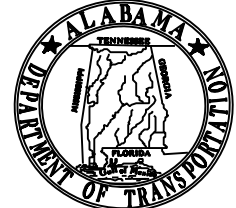


GORE DETAILS FOR RAISED CURBED ISLANDS OUTSIDE SHOULDER



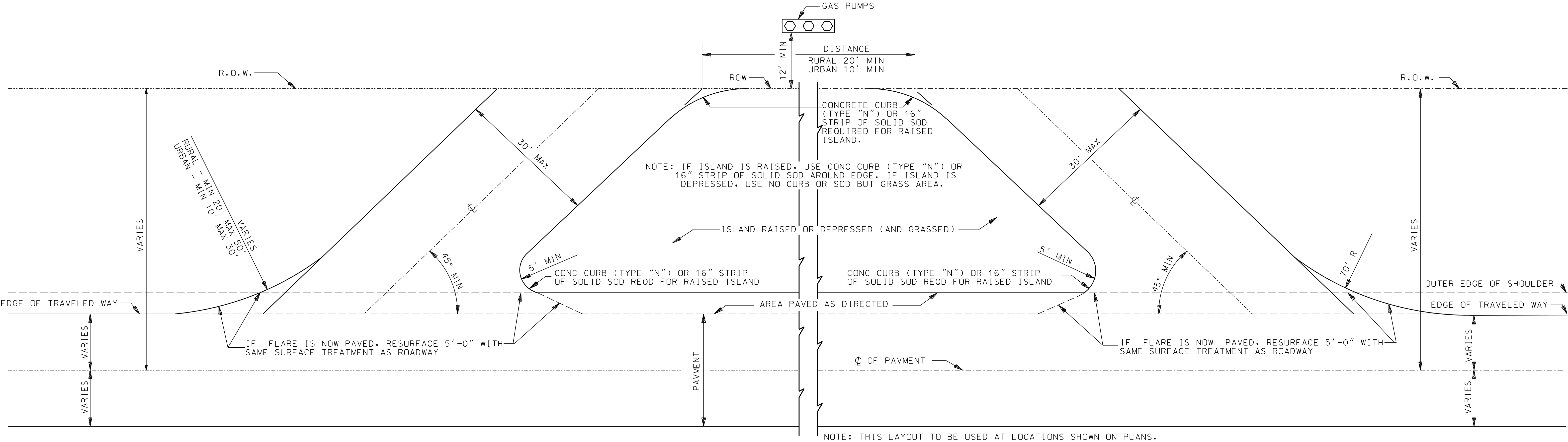
SECTION BB & FF

--SPECIFICATIONS--  
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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REVISIONS 1. Added to CADD to 08-23-00 by J.F.T. 2. Noted 5' or 6' optional stripe width on 08-23-07 by W.W.A.	 <b>ALABAMA DEPARTMENT OF TRANSPORTATION</b> 1409 COLISEUM BOULEVARD MONTGOMERY, AL 36130-3050
DESIGN BUREAU SPECIAL DRAWING <b>DETAILS OF MEDIAN OPENING AND SAFETY GORES AT TRAFFIC CHANNEL ISLANDS</b>	
Bureau Std Engr: <u>L.V.S.</u> DRAWN BY: _____ DATE DRAWN: <u>11-06-90</u>	SPECIAL DRAWING NO <b>623-N SPEC</b>
INDEX NO <b>701</b>	

NOT TO SCALE

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO




NOTE: THIS LAYOUT TO BE USED AT LOCATIONS SHOWN ON PLANS.

--SPECIFICATIONS--  
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REVISIONS

1. Added to CADD on 08-23-00  
by J.F.T.



**ALABAMA DEPARTMENT  
OF TRANSPORTATION**  
1409 COLISEUM BOULEVARD  
MONTGOMERY, AL 36130-3050

DESIGN BUREAU SPECIAL DRAWING

**TYPICAL CHANNELIZATION  
AT SERVICE STATION**

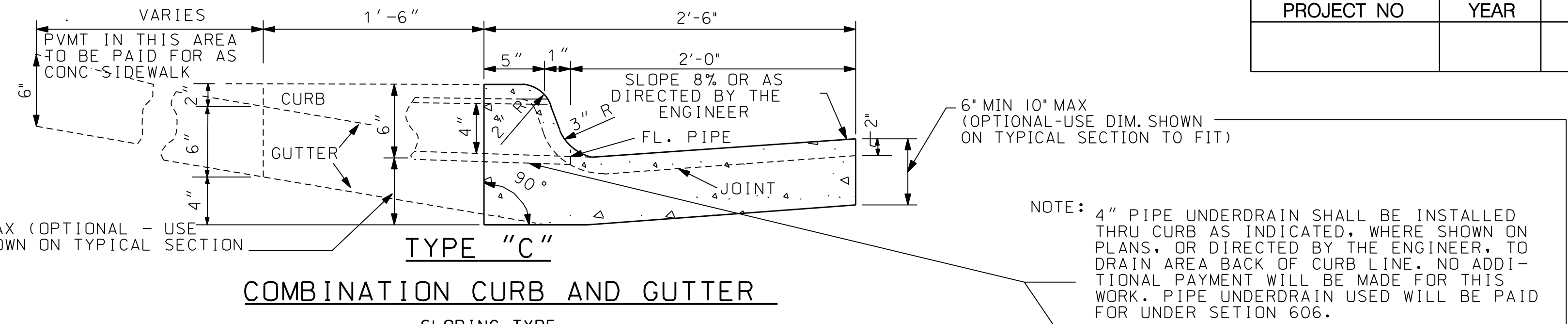
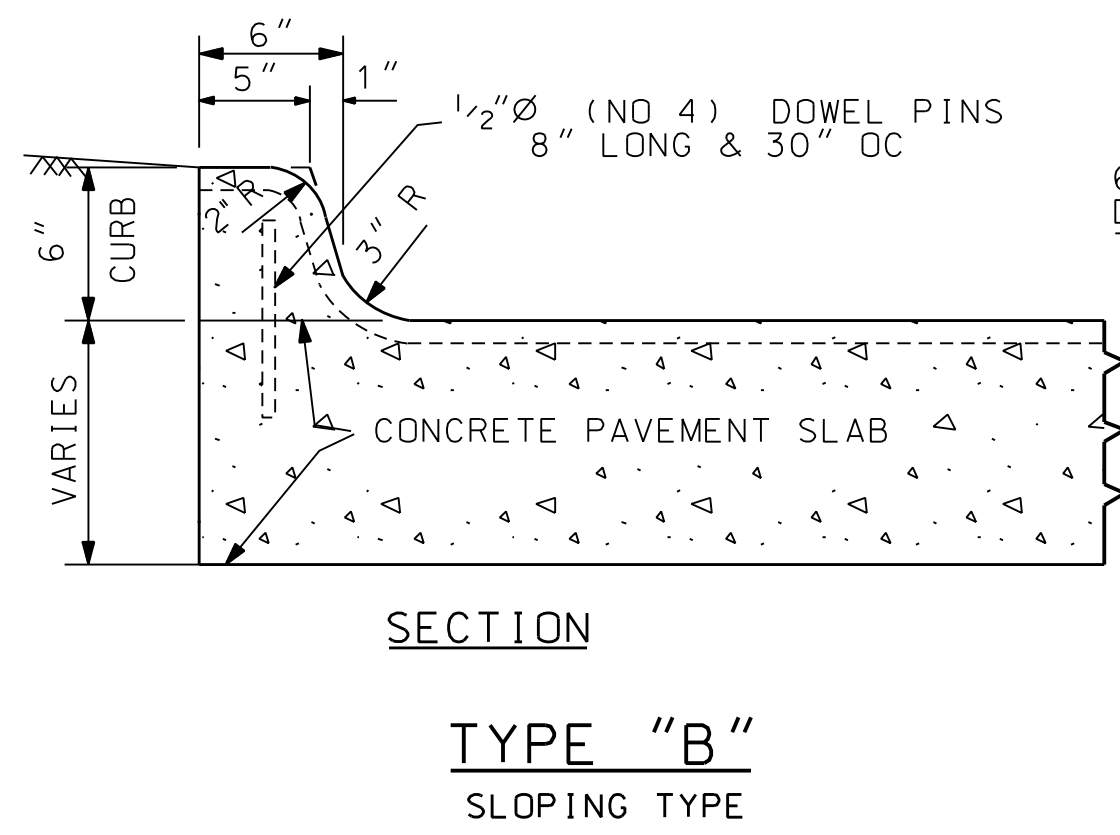
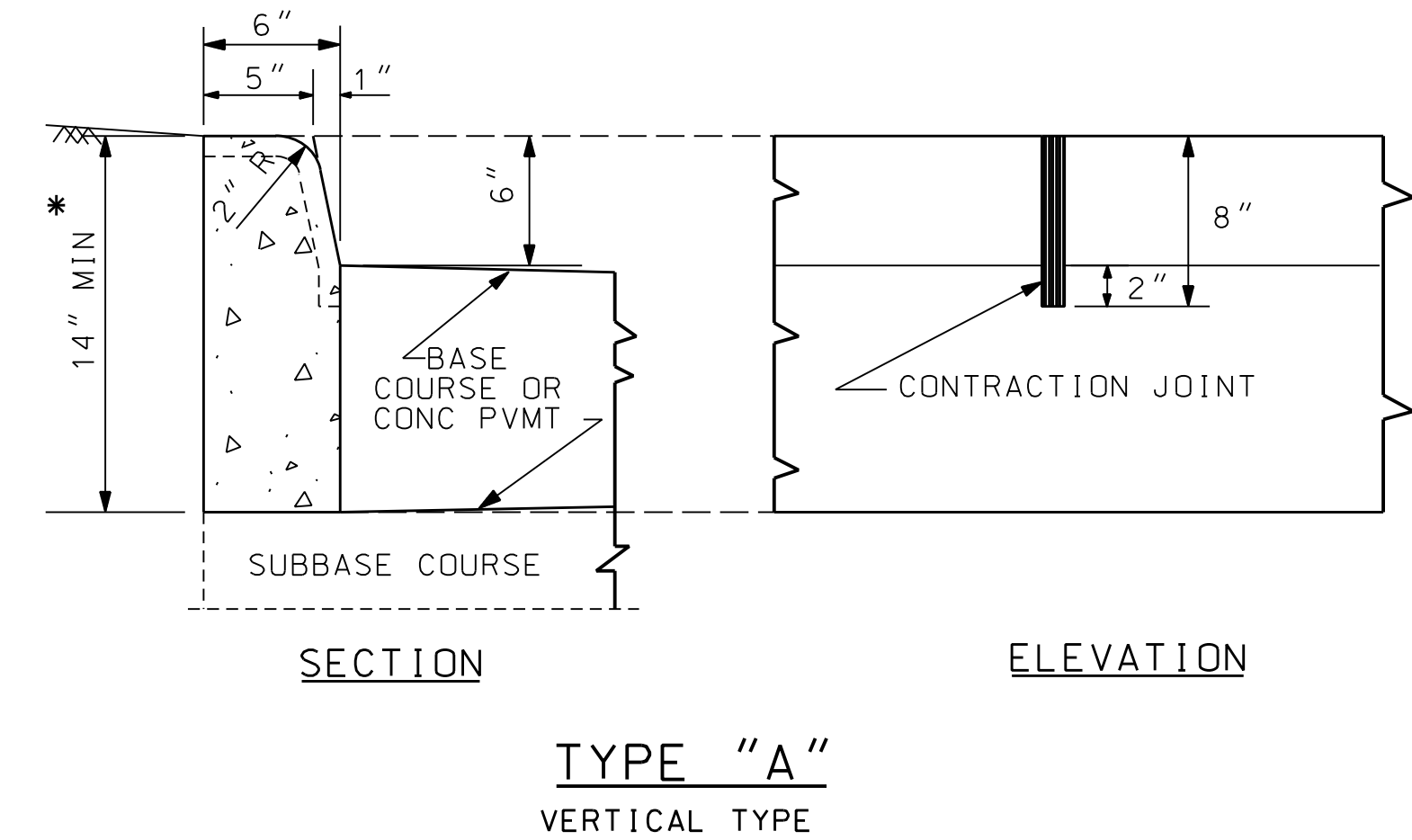
Bureau Std Engr: **L.V.S.**  
DRAWN BY: **C.J.S.** DATE DRAWN: **01-31-92**

SPECIAL DRAWING NO  
**623-SC**

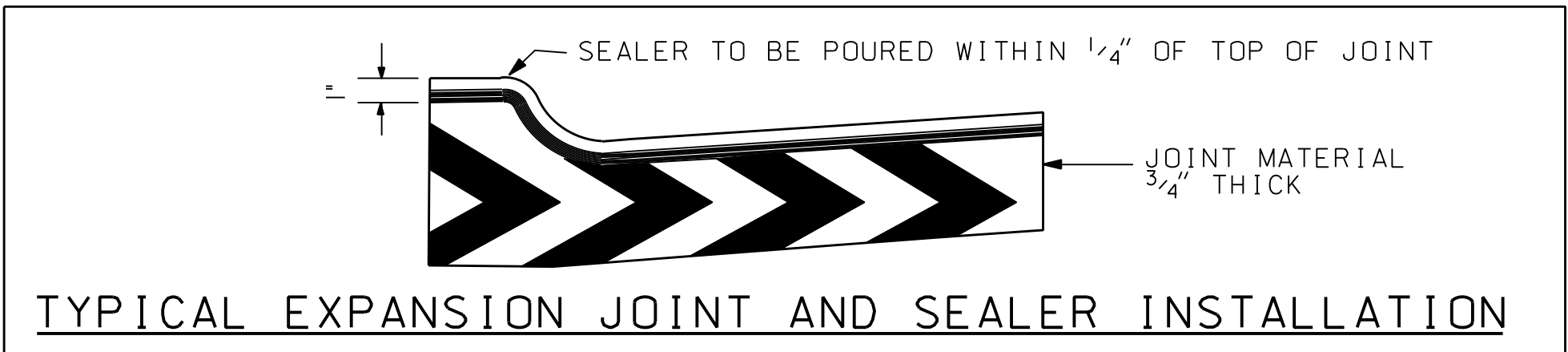
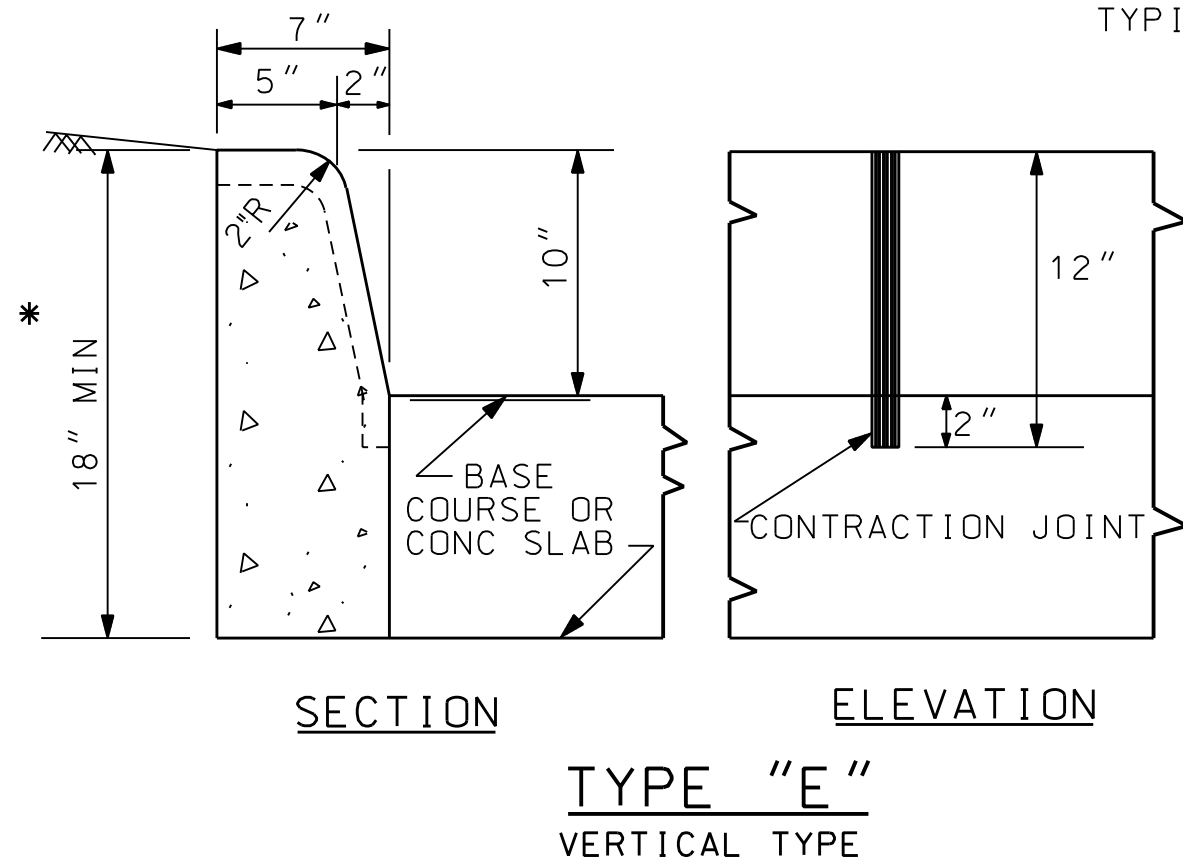
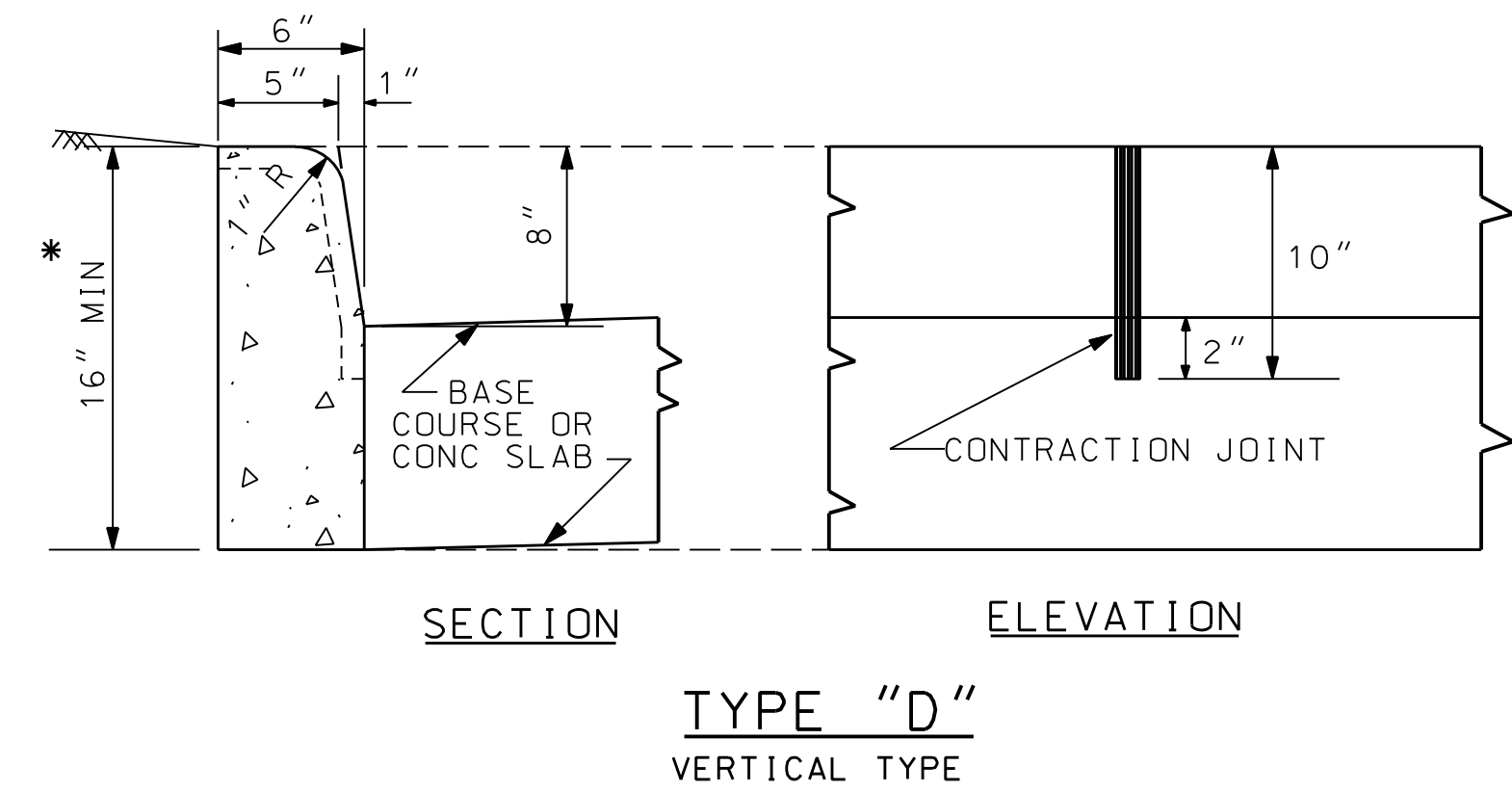
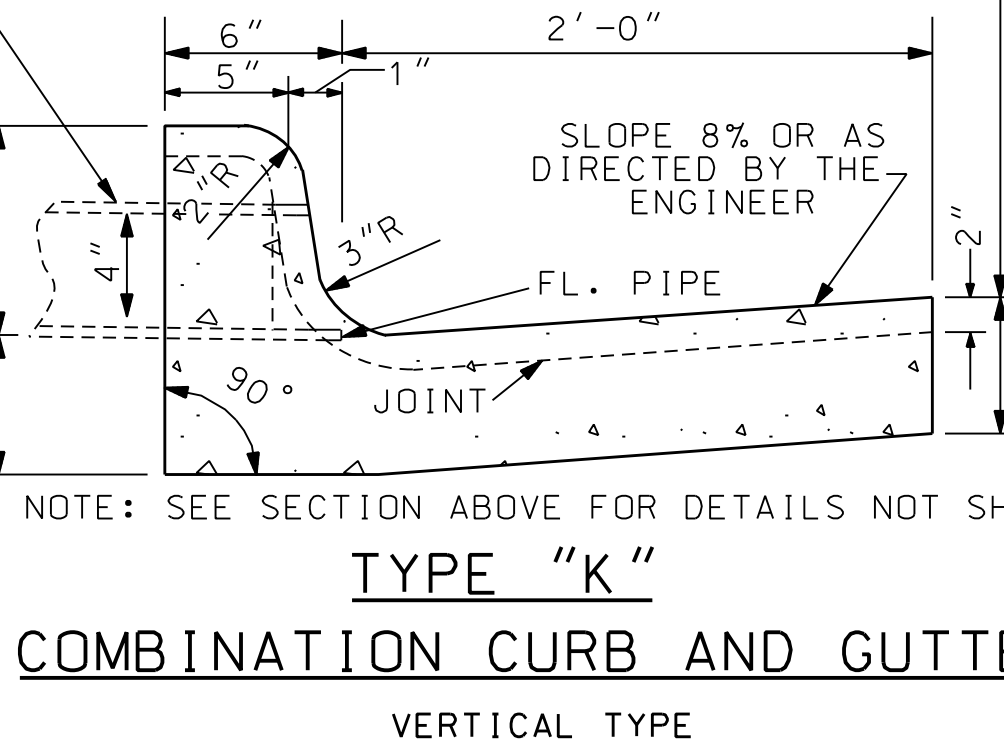
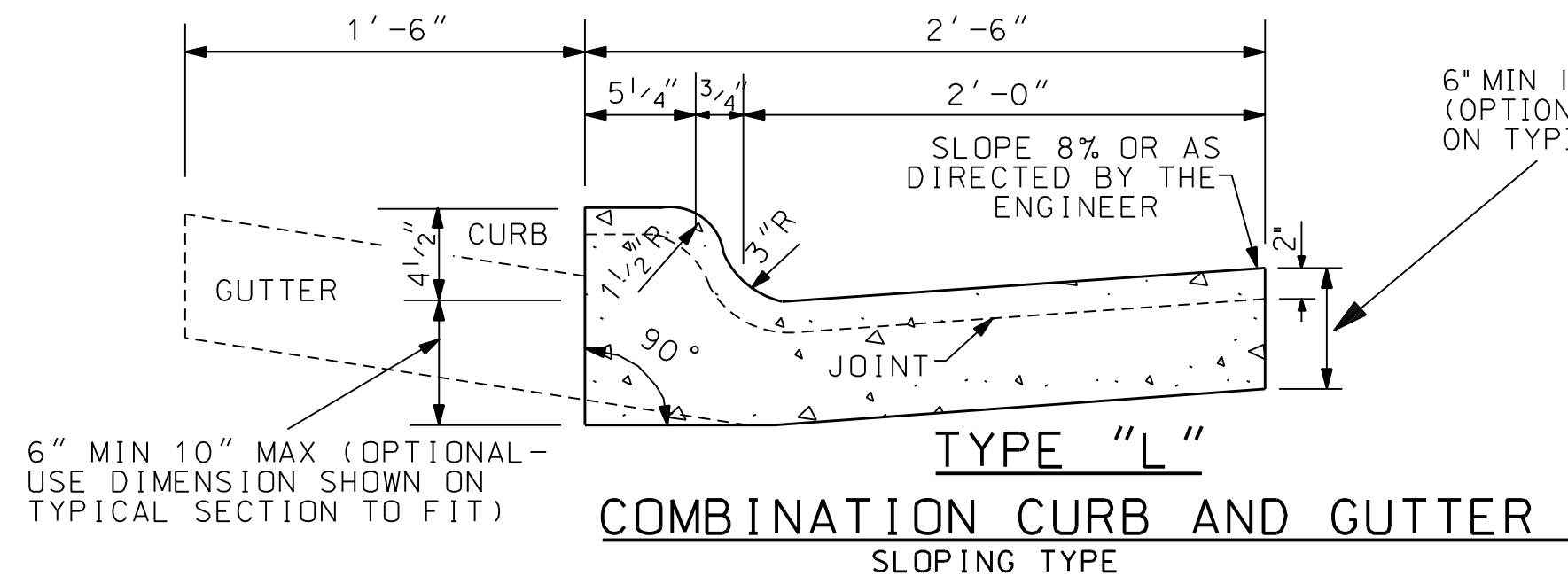
INDEX NO  
**702**

NOT TO SCALE

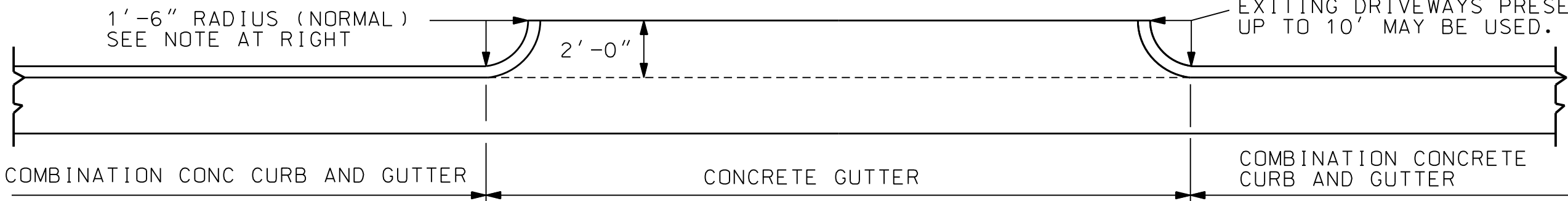
REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO



NOTE: 4" PIPE UNDERDRAIN SHALL BE INSTALLED THRU CURB AS INDICATED, WHERE SHOWN ON PLANS, OR DIRECTED BY THE ENGINEER, TO DRAIN AREA BACK OF CURB LINE. NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS WORK. PIPE UNDERDRAIN USED WILL BE PAID FOR UNDER SETION 606.



NOTE: CURB TYPE WILL BE DESIGNATED ON THE SUMMARY OF QUANTITIES SHEET.

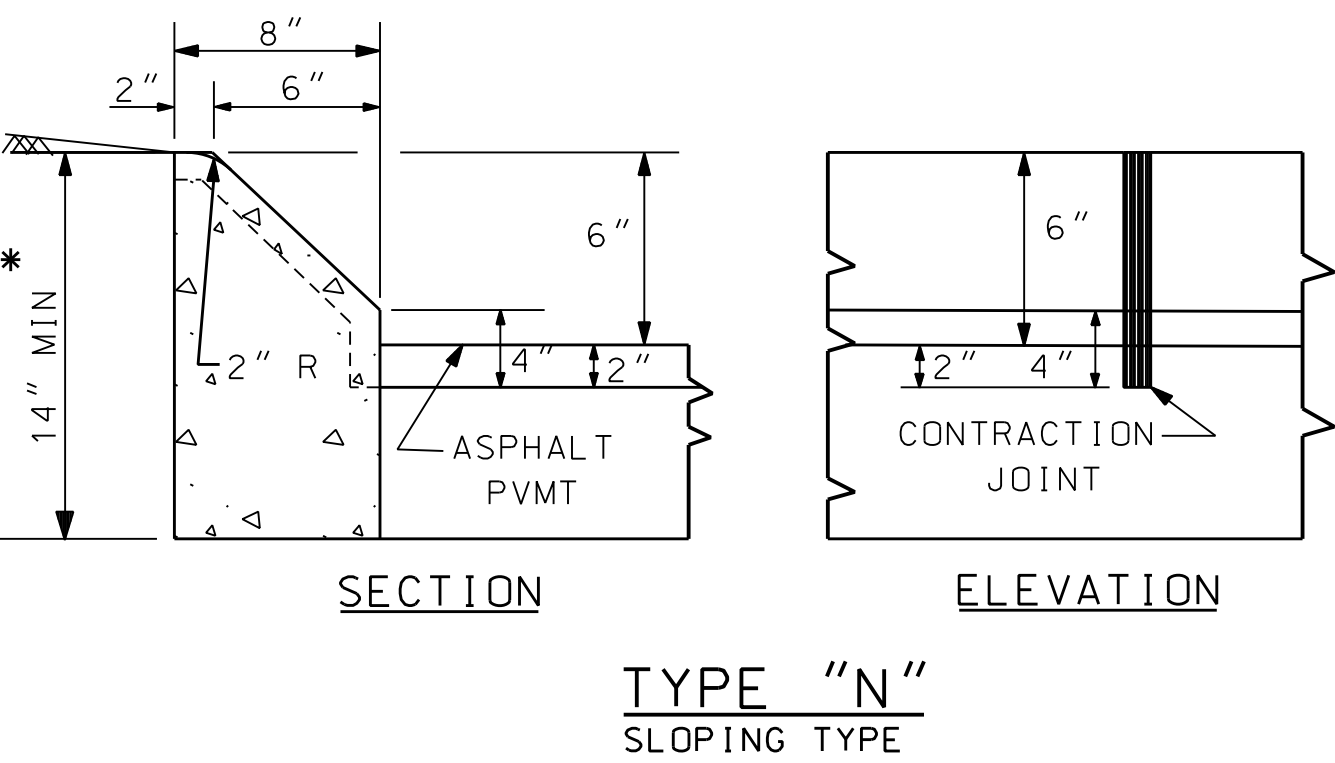
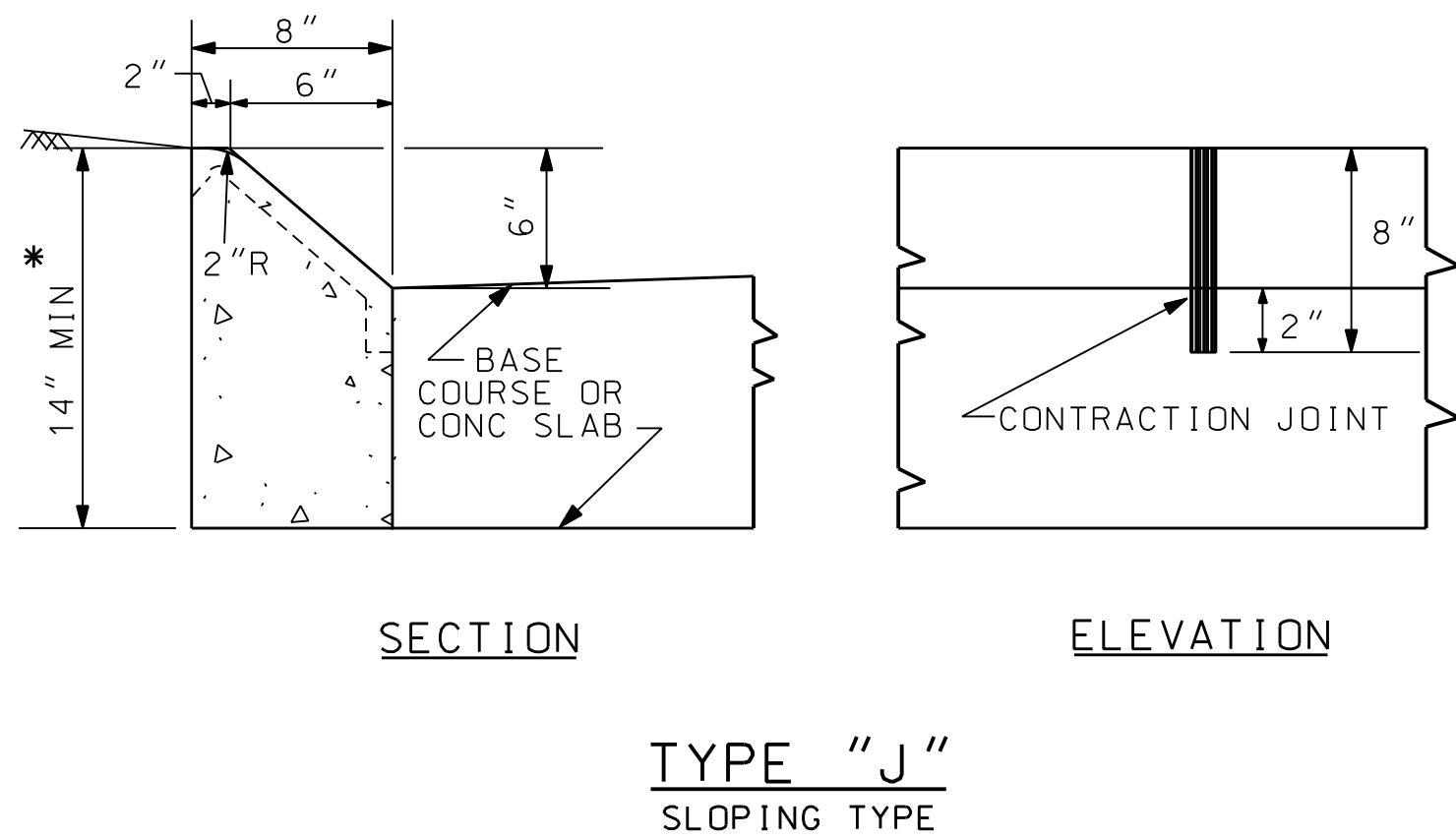
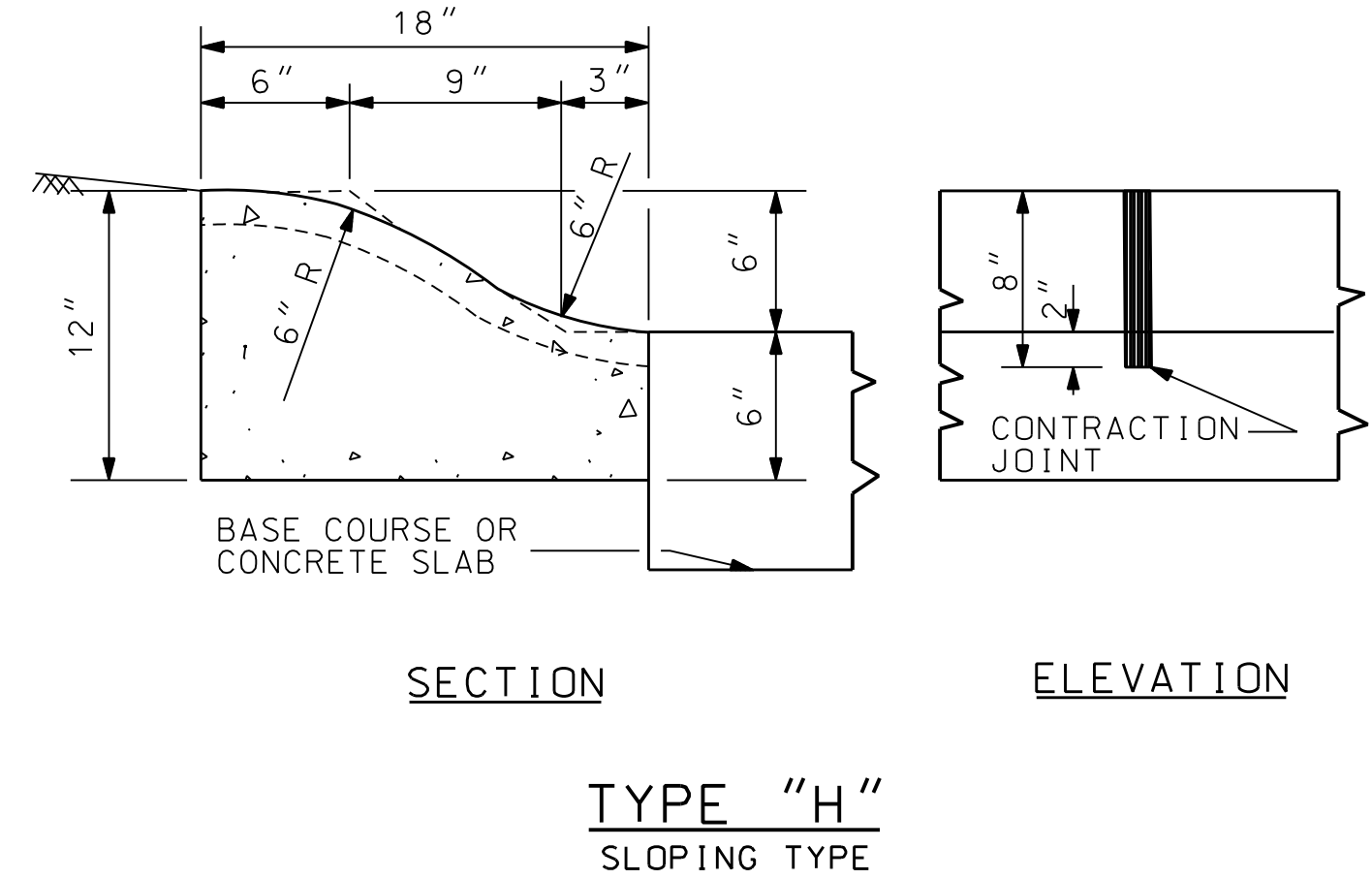
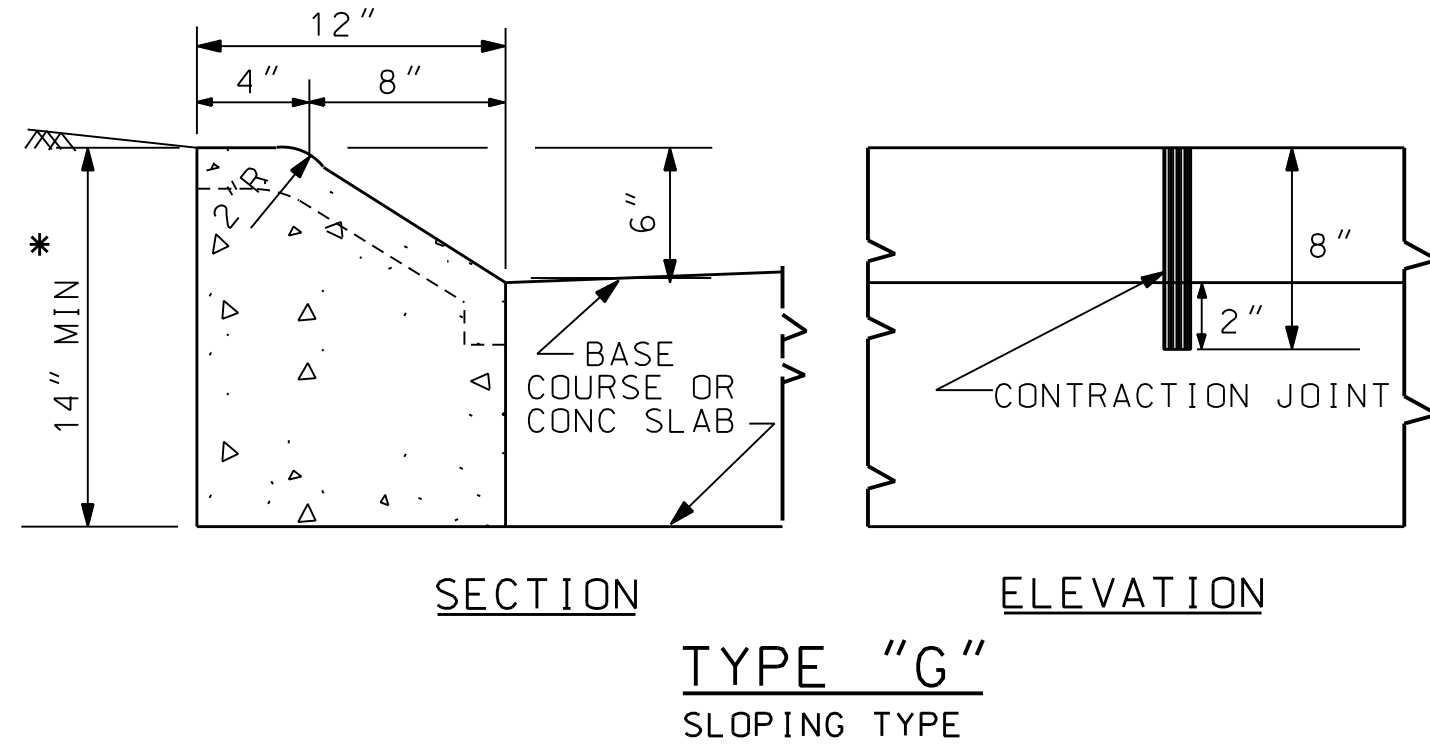
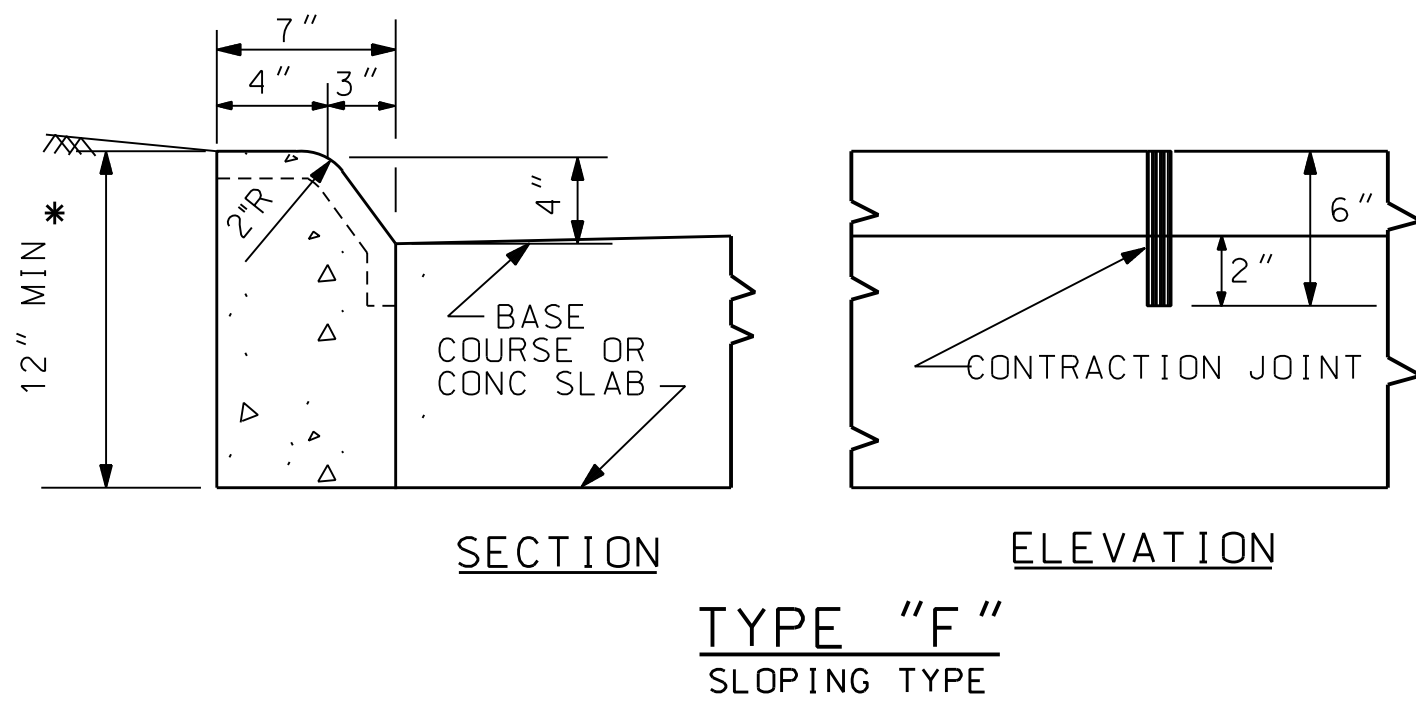


NOTE: THE LENGTH OF THE COMBINATION CONCRETE CURB AND GUTTER SHALL BE MEASURED ALONG FLOW LINE OF GUTTER. SECTION OF CONCRETE GUTTER ACROSS TURNOUTS FOR DRIVEWAYS, ETC. SHALL BE MEASURED FROM RADIUS POINT TO RADIUS POINT ALONG THE FLOW LINE OF THE GUTTER AND PRICE BID PER LINEAR FOOT SHALL INCLUDE THE RETURN CURB.

### PLAN OF COMBINATION CURB AND GUTTER AT DRIVEWAYS

#### GENERAL NOTES FOR CURB AND CURB AND GUTTER

\*1. THE CONTRACTOR MAY BE REQUIRED TO EXCEED THE MINIMUM CURB HEIGHT SHOWN IN ORDER TO PLACE CURB ON A BASE LAYER. UP TO 4" ADDITIONAL CURB HEIGHT WILL BE INCLUDED IN THE COST OF THIS STANDARD CURB.

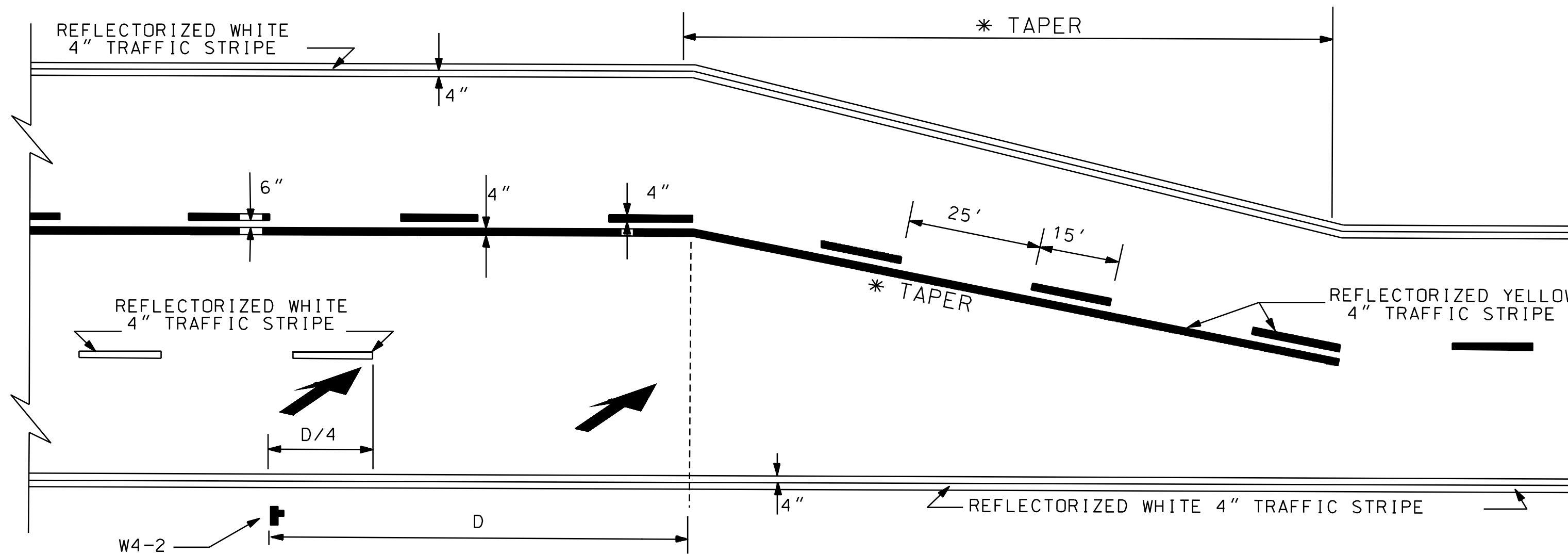


#### --SPECIFICATIONS--

#### CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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<b>REVISIONS</b> 1. Added to CADD on 03-29-99 by Joe Thomas. 2. Clarified note no.5 as to additional curb height cost on 04-24-02 by J.F.T. 3. Changed the words "MOUNTABLE" & "BARRIER" to "SLOPING" & "VERTICAL". Revised on 02-14-06 by W.W.A. 4. Individually placed type of CURB & CURB & GUTTER and deleted General Notes no.'s 1-4 on 09-23-08 by W.W.A.	<b>ALABAMA DEPARTMENT OF TRANSPORTATION</b> 1409 COLISEUM BOULEVARD MONTGOMERY, AL 36130-3050 DESIGN BUREAU SPECIAL DRAWING
Bureau Std Engr: <u>L.W.S.</u> DATE DRAWN: <u>03-31-93</u>	SPECIAL DRAWING NO <b>623-XY</b> INDEX NO <b>703</b>

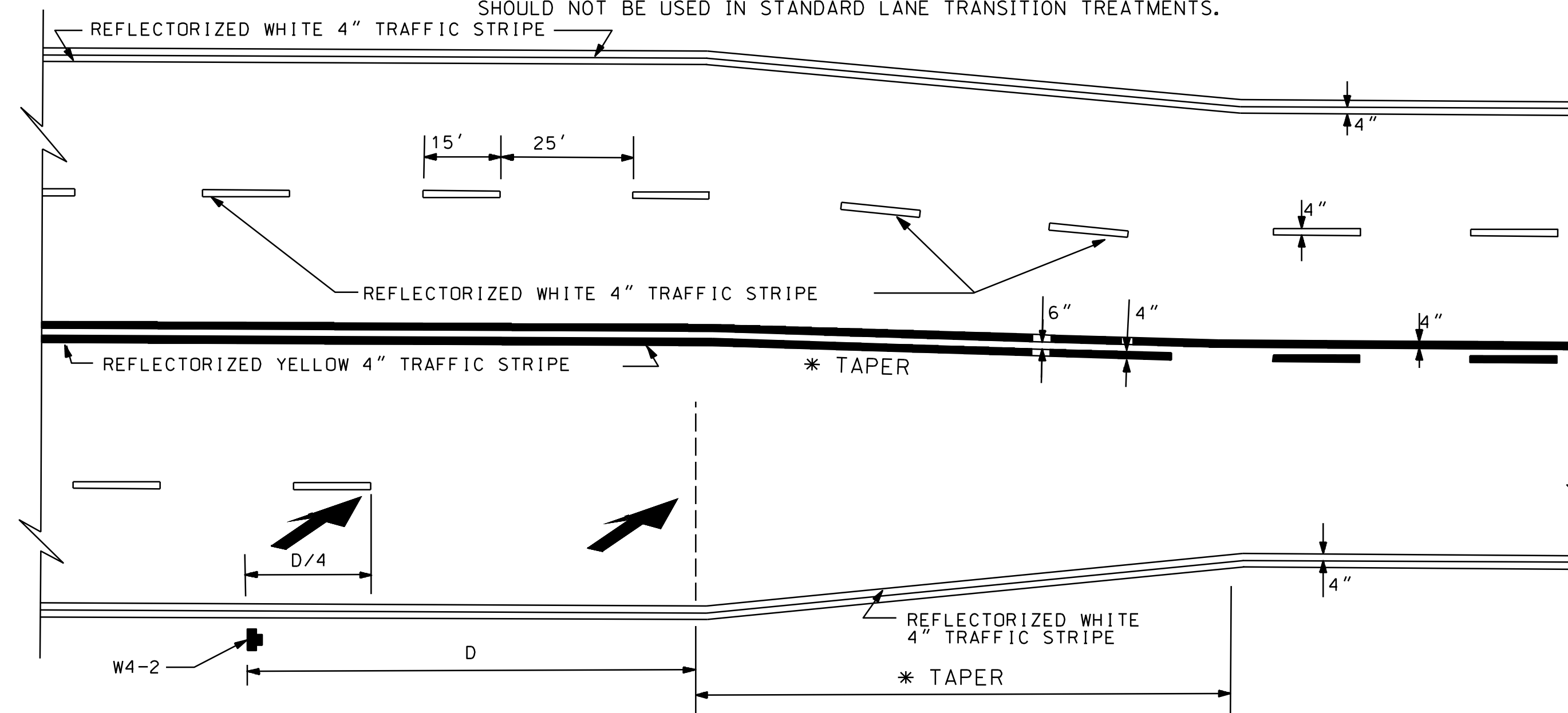
NOT TO SCALE



### TRANSITIONING FROM 3 LANES TO 2 LANES

- NOTE: 1. IF THE RECOMMENDED SPACING IN THIS TABLE DOES NOT PERMIT AT LEAST FIVE DIAGONAL LINES WITHIN THE RESTRICTED AREA, THE SPACING SHOULD BE REDUCED TO PROVIDE FOR FIVE DIAGONAL LINES. THE SPACING SHOULD NOT BE LESS THAN 4 FEET. DIAGONAL LINES SHALL BE MARKED AT A 45 DEGREE ANGLE TO THE MARKINGS OUTLINING THE RESTRICTED AREA OR TO THE CENTER LINE OF THE MAIN TRAVELWAY.
2. THE RECOMMENDED SPACING IS MEASURED PARALLEL TO THE PAVEMENT CENTER LINE, ALONG THE MARKING OUTLINING THE RESTRICTED AREA OR ALONG THE CENTER LINE OF THE MAIN TRAVELWAY.
3. THE DIAGONAL LINE SPACING FOR  $\leq 40$  SHOULD BE USED IN THE IMMEDIATE VICINITY OF INTERSECTIONS AND MAY BE USED IN OTHER AREAS WHERE ENGINEERING JUDGEMENT INDICATES THAT REDUCED SPACING IS DESIRABLE TO IMPROVE VISIBILITY.
4. STANDARD SIGNS SHOULD BE PLACED AS SHOWN AND THEN SUPPLEMENTED BY THE APPROPRIATE PAVEMENT MARKINGS. THE W9-1 (RIGHT (LEFT) LANE ENDS) MAY BE USED IN ADVANCE OF THE W4-2. IF USED, THE W9-1 SHOULD BE LOCATED AT DISTANCE 'D' UPSTREAM OF THE W4-2.
5. ONE OR MORE LANE REDUCTION ARROWS MAY BE USED IN THE FULL WIDTH PORTION OF THE TERMINATED LANE WHERE ADDITIONAL MARKING GUIDANCE IS DESIRABLE. DOTTED LINE MARKINGS SHOULD NOT BE USED IN STANDARD LANE TRANSITION TREATMENTS.

RECOMMENDED DIAGONAL LINE SPACING		
SPEED	INTERSECTION CHANNELIZATION	PAVEMENT TRANSITION
	V	Z
$\leq 40$	10'	20'
$\geq 45$	20'	30' - 40'

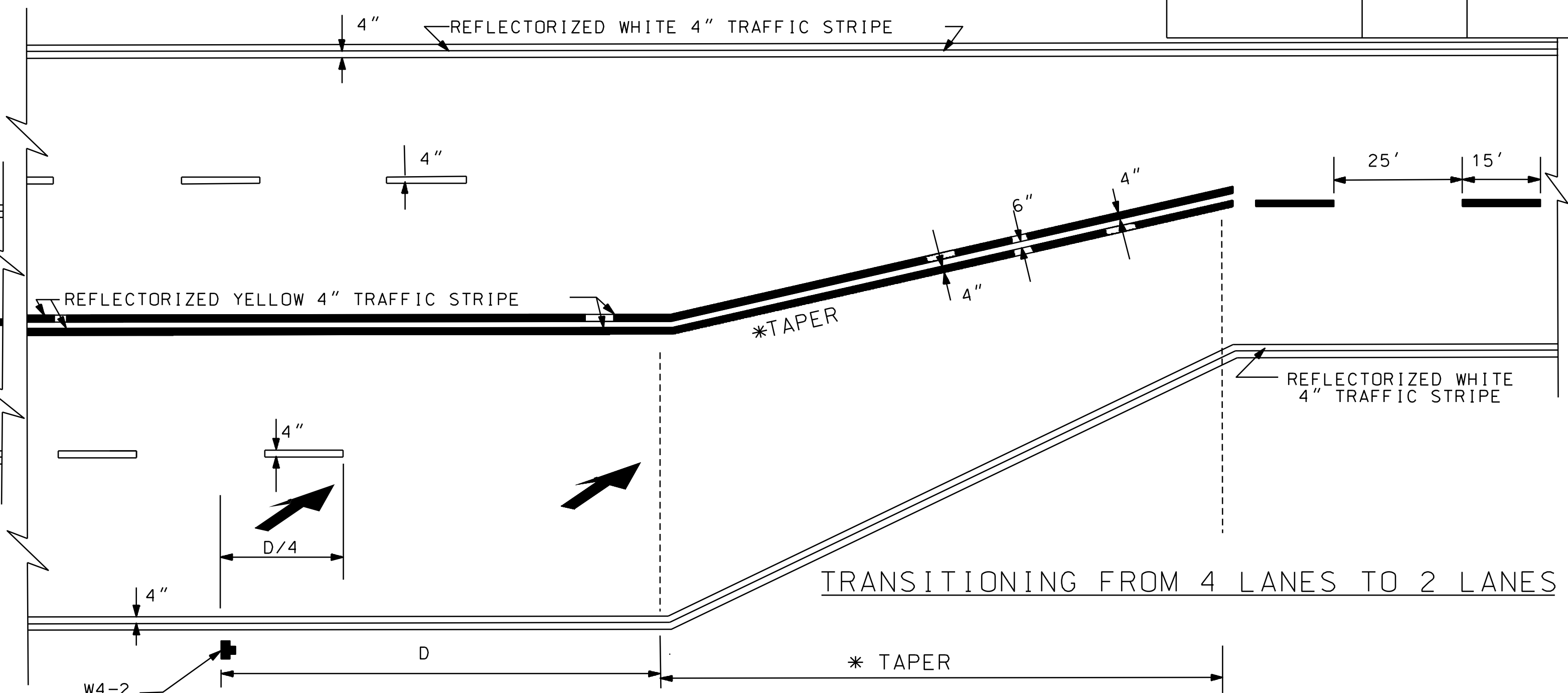


### TRANSITIONING FROM 4 LANES TO 3 LANES

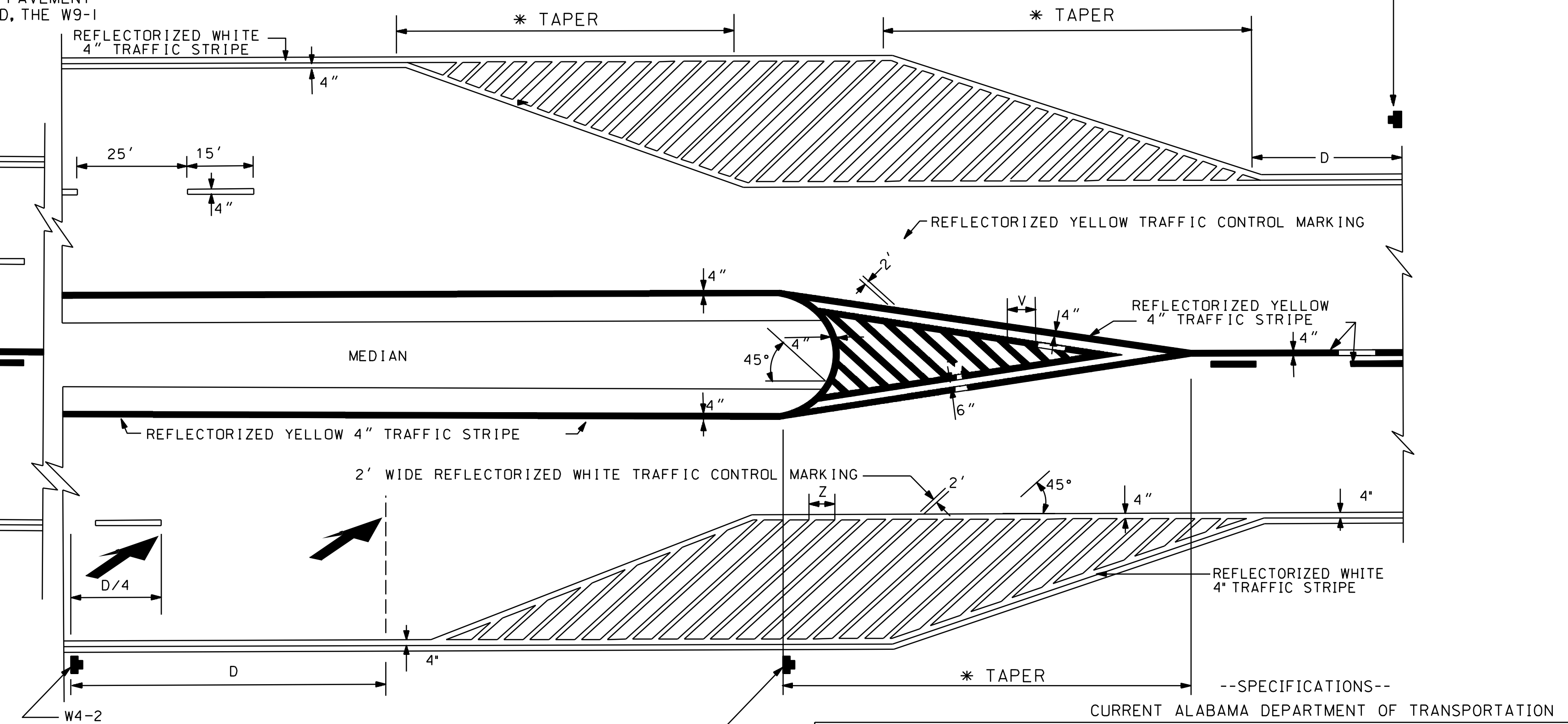
\* NOTE:  
L = LENGTH IN FEET (TAPER)  
S = 85th PERCENTILE SPEED OR POSTED SPEED  
W = OFFSET IN FEET  
FOR SPEEDS 45 MPH OR GREATER USE  $L = WS$   
FOR SPEEDS 40 MPH OR LESS USE  $L = WS^2/60$

S	D
25 mph	325'
30 mph	460'
35 mph	565'
40 mph	670'
45 mph	775'
50 mph	885'
55 mph	990'
60 mph	1100'
65 mph	1200'
70 mph	1250'

- NOTE: USE SEGMENTS OF TEN FEET FOR STRIPE AND THIRTY FEET FOR GAPS ON RURAL HIGHWAYS OR OUTSIDE CITY LIMITS.
- NOTE: THE 5" OR 6" TRAFFIC STRIPE IS OPTIONAL AT LOCATIONS SHOWN AS 4" TRAFFIC STRIPE. THE PAYMENT OF THE 5" OR 6" TRAFFIC STRIPE WILL BE IDENTIFIED IN THE PAY ITEM USED.



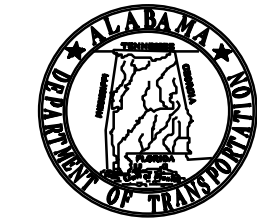
### TRANSITIONING FROM 4 LANES TO 2 LANES



### TRANSITIONING FROM 4 LANES TO 2 LANES

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- REVISIONS
- Added to CADD on 11-02-98 by Joe Thomas
  - Revised 'D' length and Diagonal line spacing based on speed and added Notes 4 & 5 on 07-13-07 by W.W.A.
  - Added Lane-reduction arrows to all various Lanes to Lanes Transitioning & updated Distances for 'D' on 08-26-14 by J.F.T.



ALABAMA DEPARTMENT OF TRANSPORTATION  
1409 COLISEUM BOULEVARD  
MONTGOMERY, AL 36130-3050

DESIGN BUREAU SPECIAL DRAWING

STANDARD TRANSITION TAPERS FOR  
2, 3 AND 4 LANE HIGHWAYS

Bureau Std Engr: J.V.S.  
DRAWN BY: D.A.K. DATE DRAWN: 10-24-83

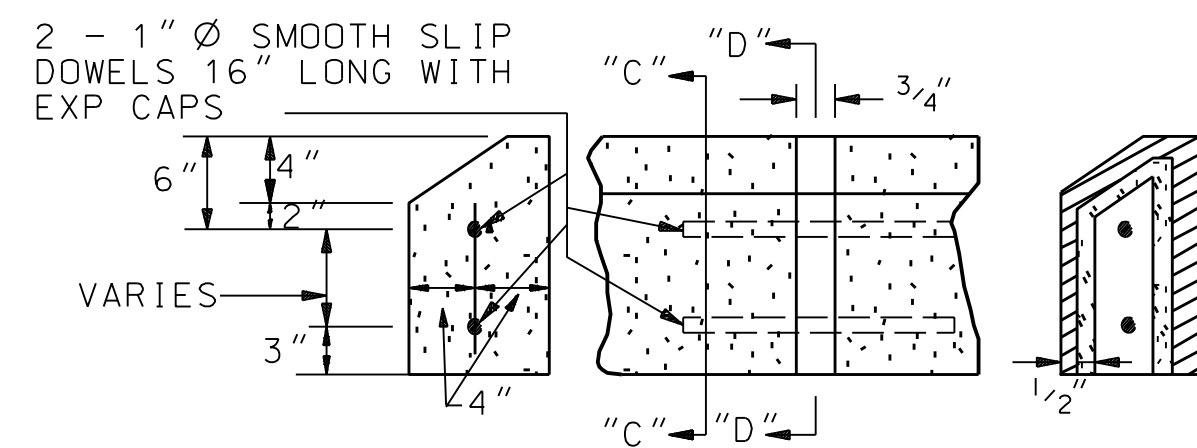
SPECIAL DRAWING NO  
LT-703

INDEX NO  
711

NOT TO SCALE



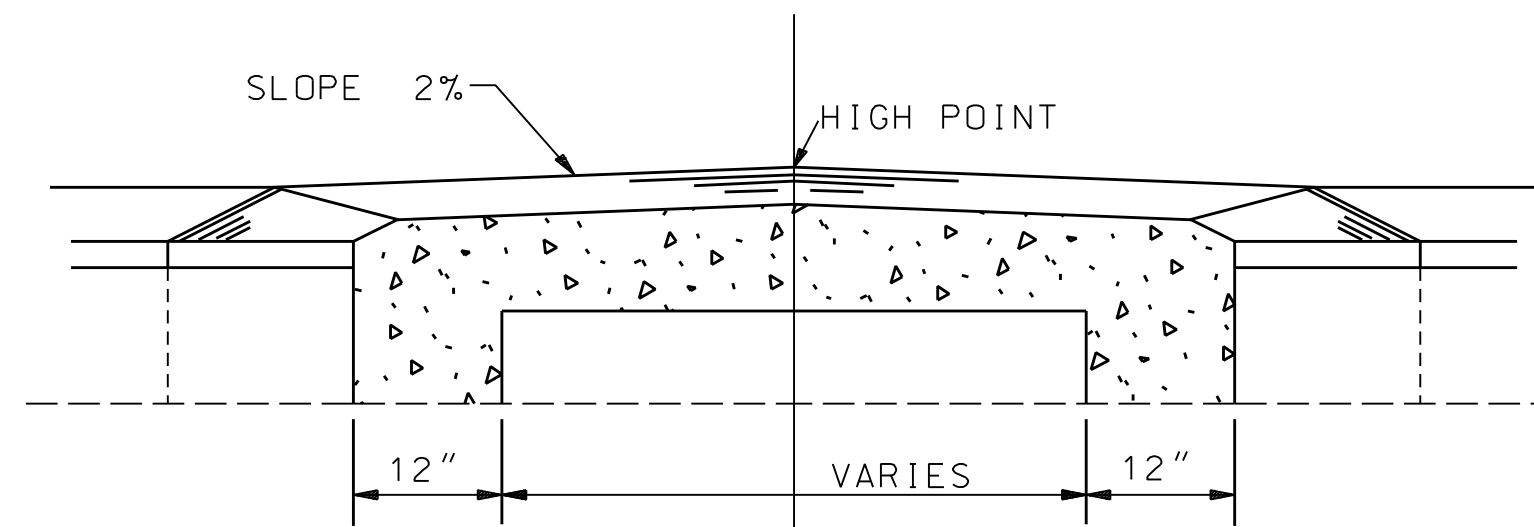
REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO



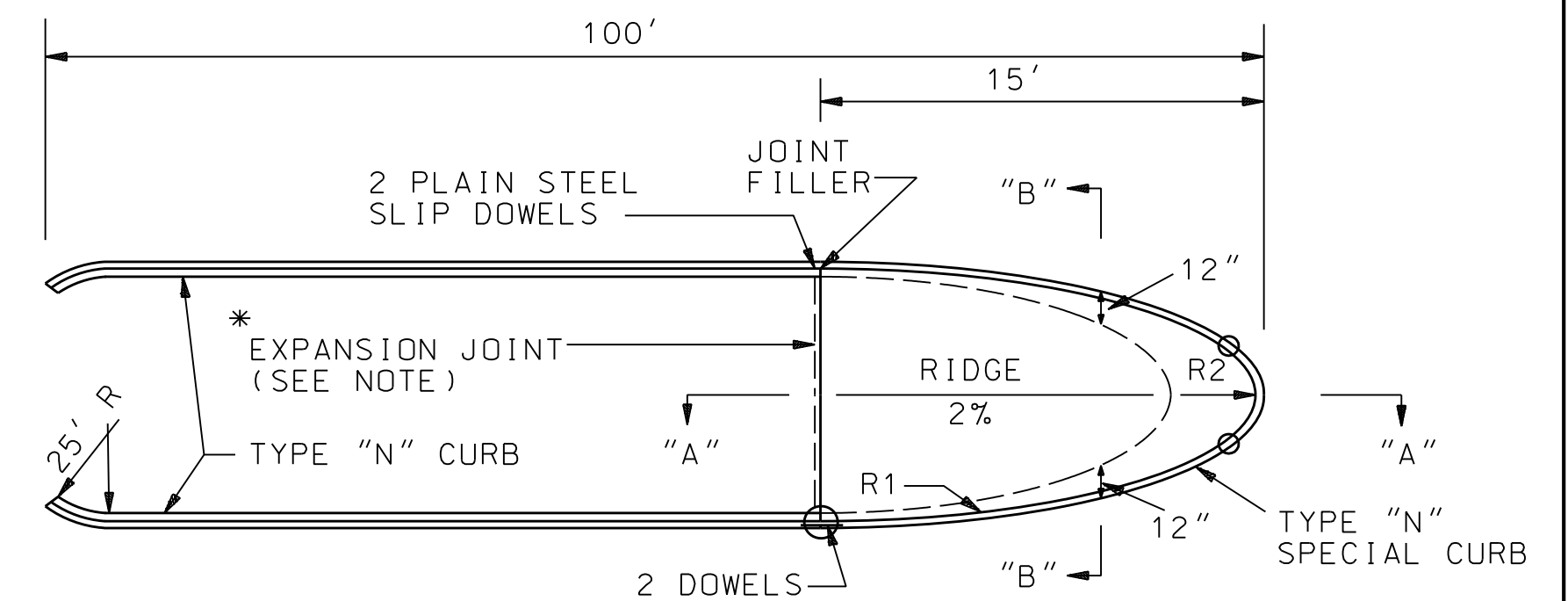
SECTION "CC"    FRONT ELEV    SECTION "DD"

DETAILS OF EXPANSION JOINT

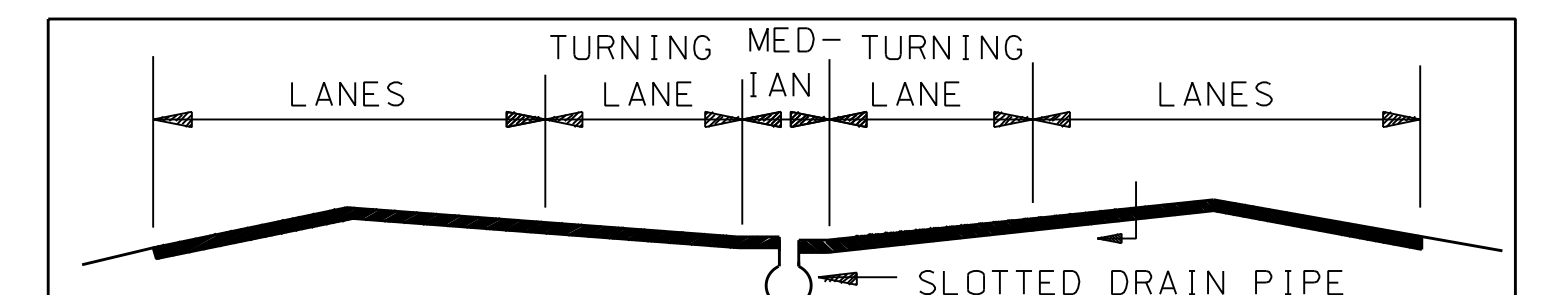
\* IF MEDIAN AREA IS PAVED BEYOND THE 15' LENGTH AS SHOWN, AND MEDIAN WIDTH IS 16' OR WIDER, AN EXPANSION JOINT AT CENTER LINE WILL BE REQUIRED FROM THAT POINT ON.



SECTION "BB"



TOP VIEW OF NOSE CURB



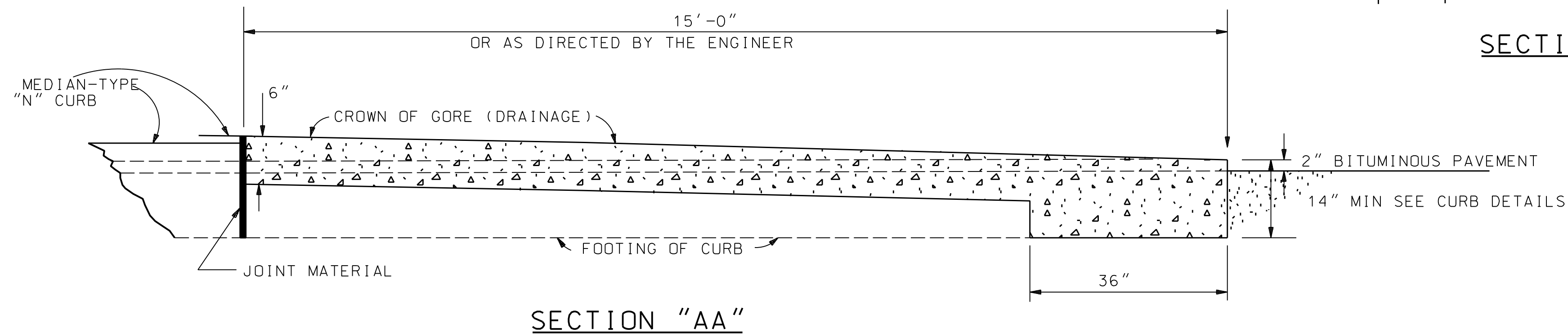
SECTION "EE"

DETAIL OF SLOTTED DRAIN PIPE 12"

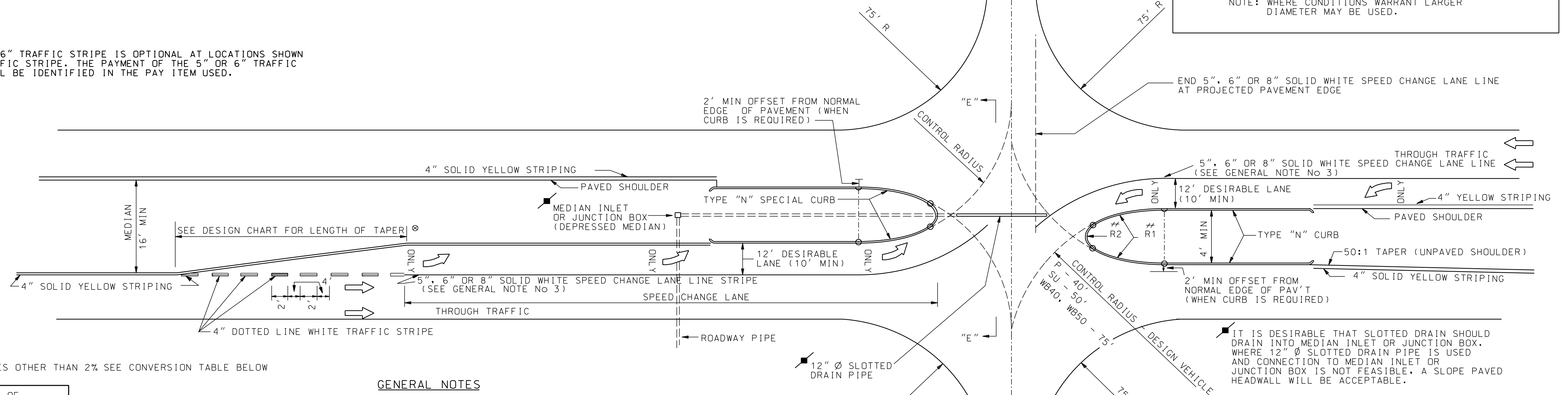
NOTE: WHERE CONDITIONS WARRANT LARGER  
DIAMETER MAY BE USED.

Note

THE 5" OR 6" TRAFFIC STRIPE IS OPTIONAL AT LOCATIONS SHOWN AS 4" TRAFFIC STRIPE. THE PAYMENT OF THE 5" OR 6" TRAFFIC STRIPE WILL BE IDENTIFIED IN THE PAY ITEM USED.



SECTION "AA"



NOTE: FOR GRADES OTHER THAN 2% SEE CONVERSION TABLE BELOW

### GENERAL NOTES

1. FOR OTHER APPLICABLE TRAFFIC STRIPING AND MARKINGS SEE APPROPRIATE STANDARD DRAWINGS AND MUTCD.
  2. PAYMENT AND MEASUREMENT OF CONC NOSE WILL BE MADE UNDER 1" CURB TYPE "N" (SPECIAL) PER LINEAR FOOT. THE GORE SECTION OF TYPE "N" (SPECIAL) SHALL BE MEASURED FROM END OF TYPE "N" AT THE JOINT AROUND THE GORE IN THE GUTTER TO THE OTHER JOINT.
  3. WHEN THE PROJECT REQUIRES THE OPTIONAL 5" OR 6" TRAFFIC STRIPE, THE SPEED CHANGE LANE LINE WILL BE THE SAME 5" OR 6" SOLID WHITE TRAFFIC STRIPE, WHEN THE PROJECT REQUIRES THE 4" TRAFFIC STRIPE, THE STRIPE WILL BE TWO (2) OF THE SAME 4" SOLID WHITE TRAFFIC STRIPE, PER MILE/LIN FT.
  4. FILLER AND SEALER FOR EXPANSION JOINTS SHALL BE AS OUTLINED IN ARTICLES 832.01 AND 832.02 ALABAMA DOT SPECIFICATIONS.
  5. A CORRUGATED SLOTTED DRAIN PIPE WILL BE REQUIRED WHERE PONDING MAY TAKE PLACE IN A VERTICAL SAG OR FLAT GRADE.
  6. FOR HIGH SPEED (50 MPH AND FASTER) RURAL DESIGNS, NO CURB AND NOSE SHALL BE USED. THE GORE SHALL CONFORM TO SAME GEOMETRIC DESIGN EXCEPT THAT INSIDE SHOULDER PAVEMENT SHALL BE EXTENDED AROUND EDGES OF CROSSOVER. IN THIS CASE THE DRAINAGE PROBLEM WILL BE ELIMINATED AND WILL NOT REQUIRE SLOTTED DRAINS.
  7. FOR HIGH SPEED RURAL DESIGNS WHEN NO CURB IS USED, THE 4" SOLID YELLOW STRIPING IN THE MEDIAN WILL BE EXTENDED AROUND THE MEDIAN GORE AREA.
- $$\frac{R_2}{R_1} = \frac{1/2 \text{ MEDIAN W}}{1/5 \text{ OF MEDIAN W}}$$

$$R_1 = \text{TURNING SPEED}$$

DESIGN LENGTH OF SPEED CHANGE LANES 2% GRADES OR LESS		
DESIGN SPEED OF HIGHWAY	LENGTH OF TAPER (ft)	STOP CONDITION INCLUDES TAPER
40	190	325
50	230	425
60	270	500
65	290	550
70	300	600
75	315	650
80	330	700
DESIGN SPEED OF HIGHWAY MPH		RATIO OF LENGTH OF
ALL		3 TO 4% UP
ALL		5 TO 6% UP

⊗ RATIO FROM THIS TABLE MULTIPLIED BY  
LENGTH GIVES LENGTH OF SPEED CHANGE  
LANE ON GRADE.

```
## R2 = 1/2 MEDIAN WIDTH (MAX) BUT IT IS PLEASING IN
PROPORTION AND APPEARANCE WHEN R2 IS APPROXIMATELY
1/5 OF MEDIAN WIDTH.
```

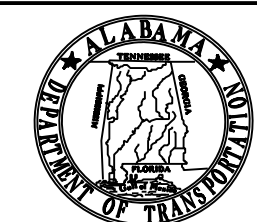
R1 = TURNING SPEED RADIUS: 20 MPH = 90', 25 MPH = 150'  
30 MPH = 230'. R1 SHALL BE A MIN OF 80'.

--SPECIFICATIONS--  
 CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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## REVISIONS

1. Added to CADD on 11-02-98 by Joe Thomas
2. Changed 8" Channelizing line to 6" or 8" Channelizing Stripe added and revised General Note No. 3 on 09-20-00 by J.F.T.
3. Corrected the dotted line dimensions on 10-23-03 by W.W.A.
4. Adjusted the optional stripe to 5' or 6' & speed change lane line width on 08-08-07 on W.W.A.



**ALABAMA DEPARTMENT  
OF TRANSPORTATION**  
1409 COLISEUM BOULEVARD  
MONTGOMERY, AL 36130-3050

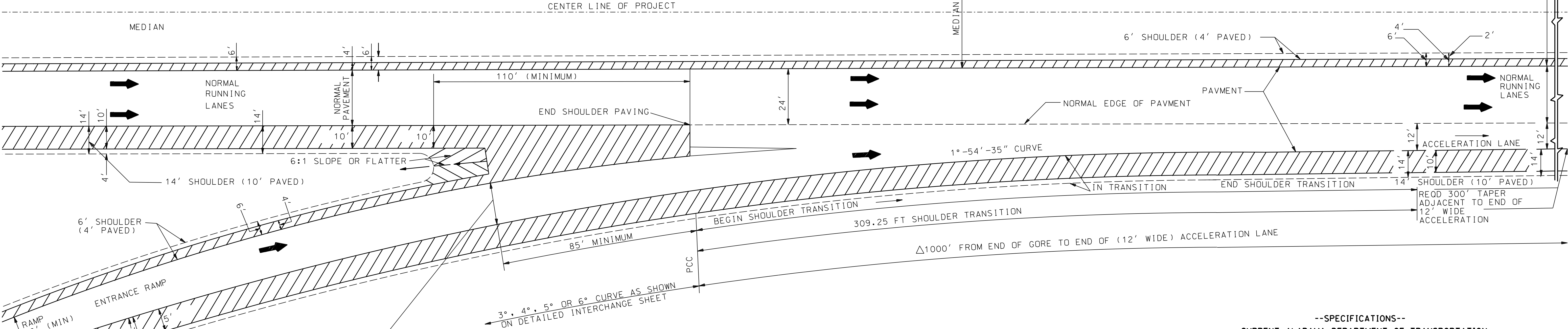
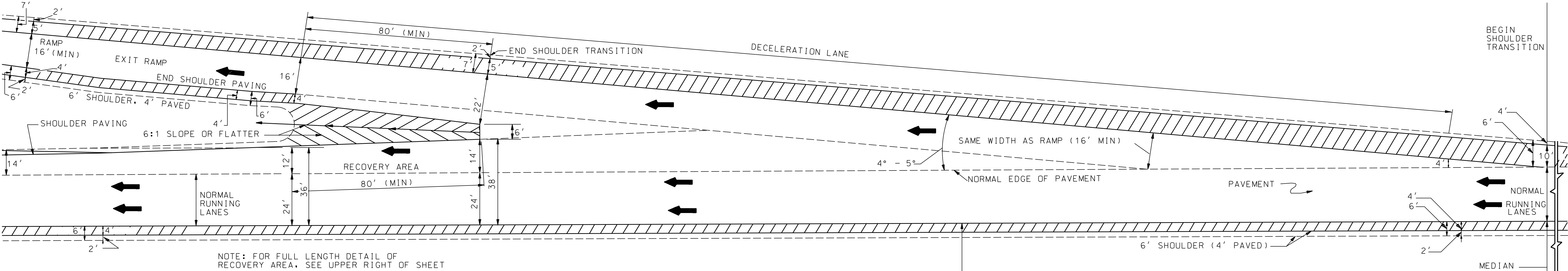
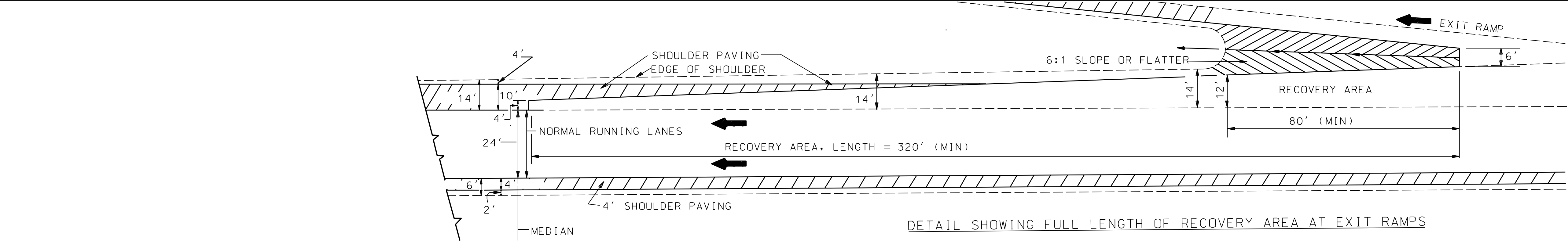
DESIGN BUREAU SPECIAL DRAWING  
GEOMETRIC DESIGN AND STRIPING  
DETAILS FOR SPEED CHANGE LANES  
WITH MEDIAN CROSSEOVERS WITH  
AND WITHOUT CURBED GORE AREAS

SPECIAL DRAWING NO  
LTL-623

DEX NO  
715

NOT TO SCALE

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO



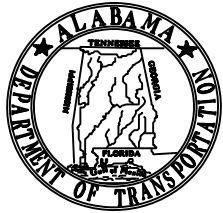
Δ1200' WHEN THE COMBINED VOLUME OF THE RAMP AND THE ADJACENT LANE OF THE MAIN LINE REACHES OR EXCEEDS 1500 VPH.

--SPECIFICATIONS--  
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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REVISIONS

1. Added to CADD on 09-08-00 by J.F.T.



**ALABAMA DEPARTMENT  
OF TRANSPORTATION**  
1409 COLISEUM BOULEVARD  
MONTGOMERY, AL 36130-3050

DESIGN BUREAU SPECIAL DRAWING

DETAILS OF GORE AT TERMINALS OF  
ENTRANCE AND EXIT RAMP FOR  
USE ON RURAL OR URBAN SECTIONS

Bureau Std Engr: L.V.S.  
DATE DRAWN: 2-07-80

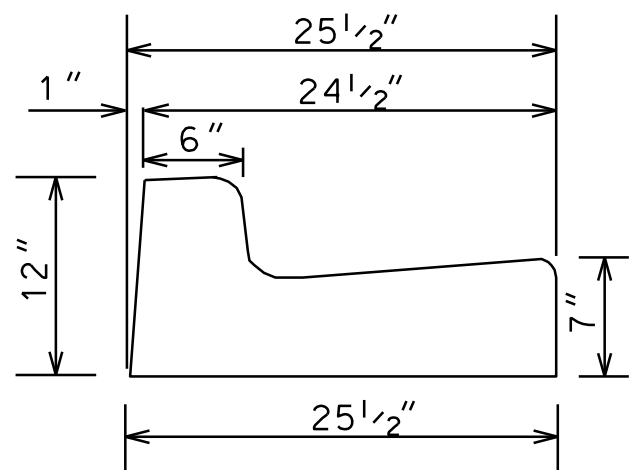
SPECIAL DRAWING NO  
**NC-623**

INDEX NO  
**723**

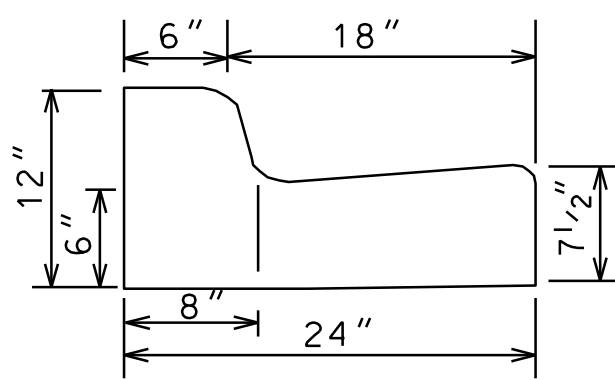
NOT TO SCALE

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

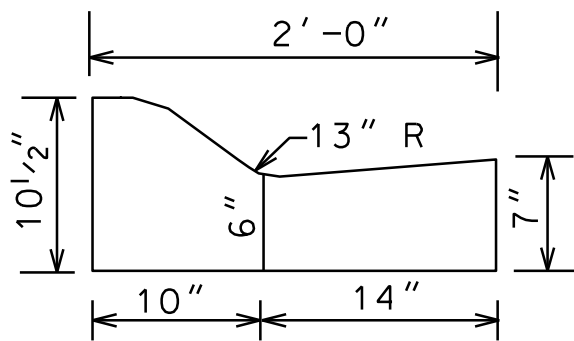
SKETCHES OF CURBS AND GUTTER  
TO BE PLACED WITH AUTOMATIC  
CURB AND GUTTER MACHINES



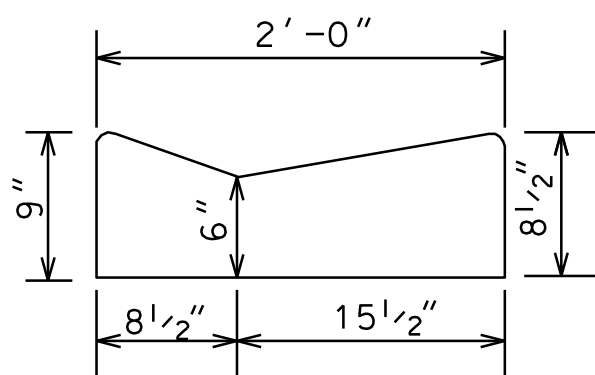
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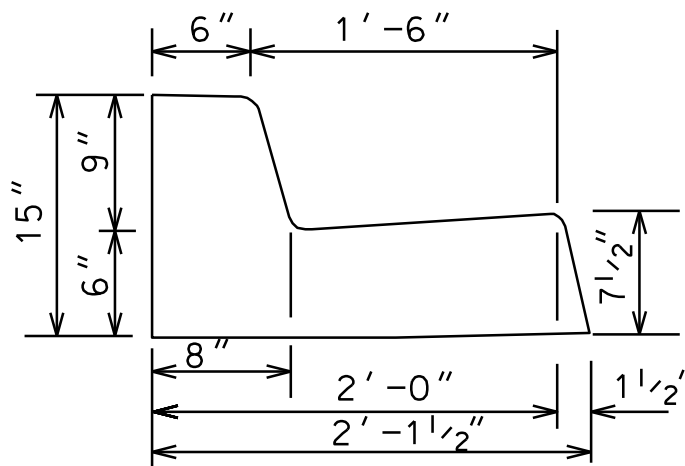
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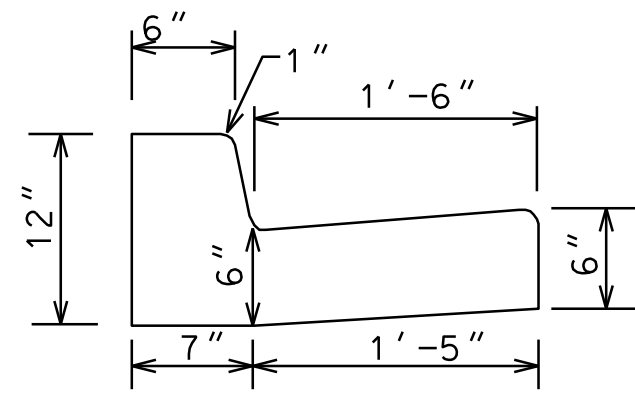
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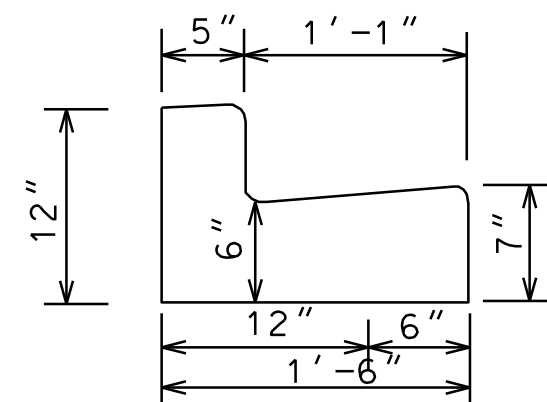
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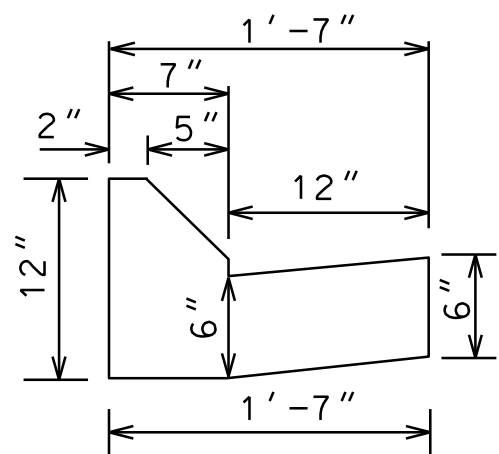
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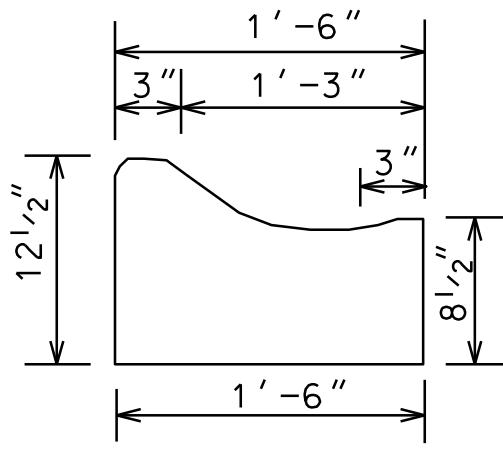
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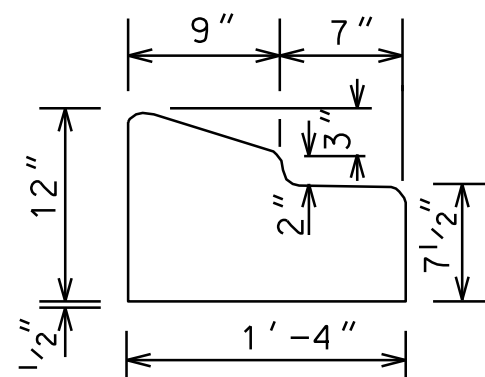
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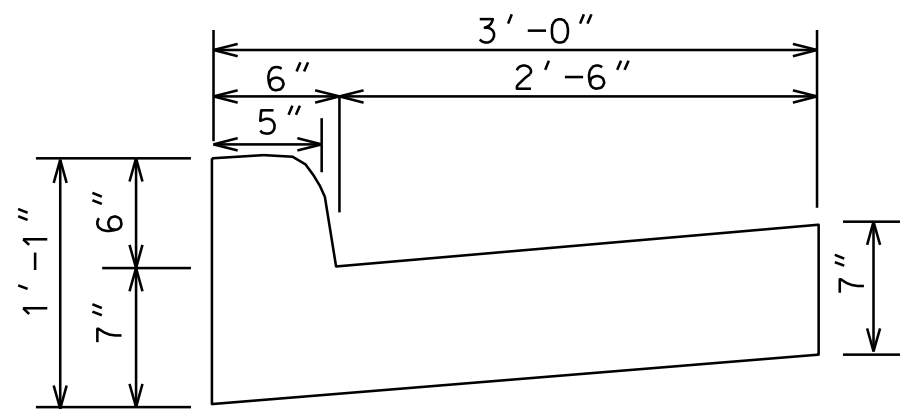
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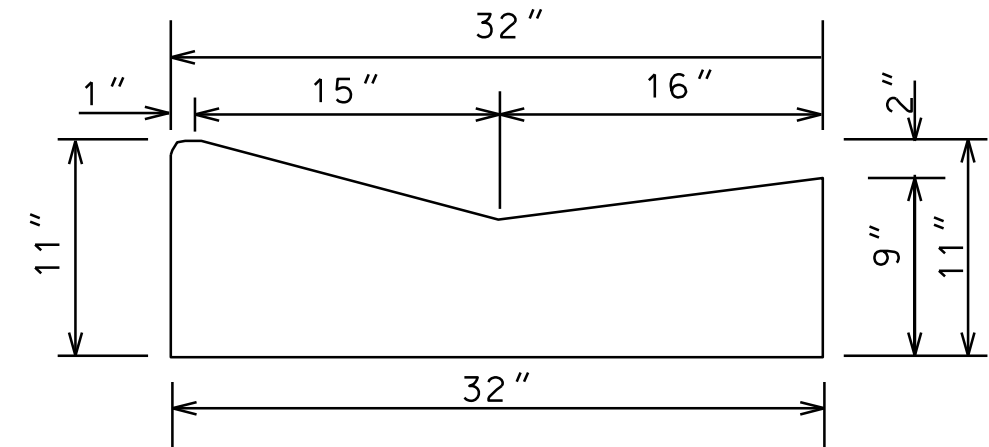
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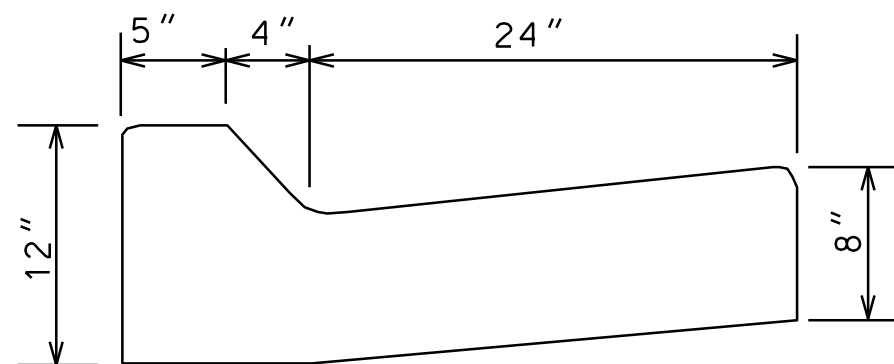
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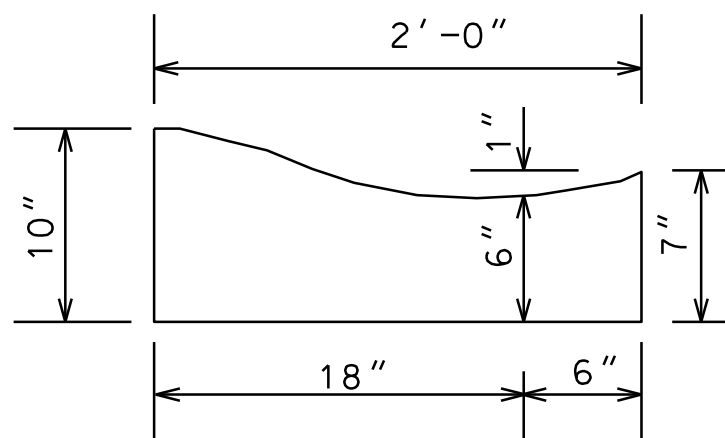
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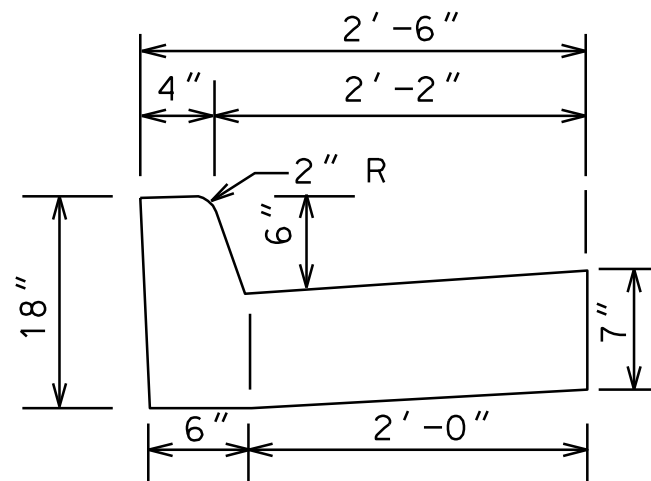
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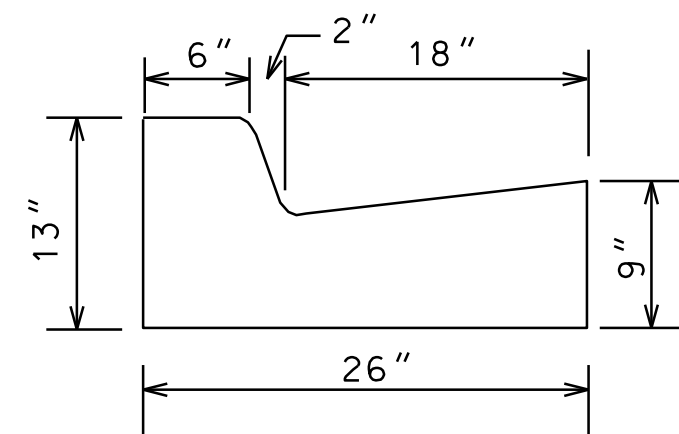
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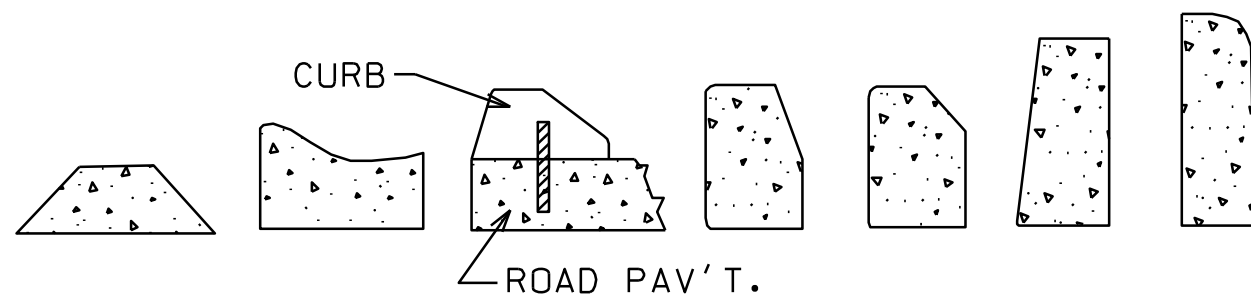
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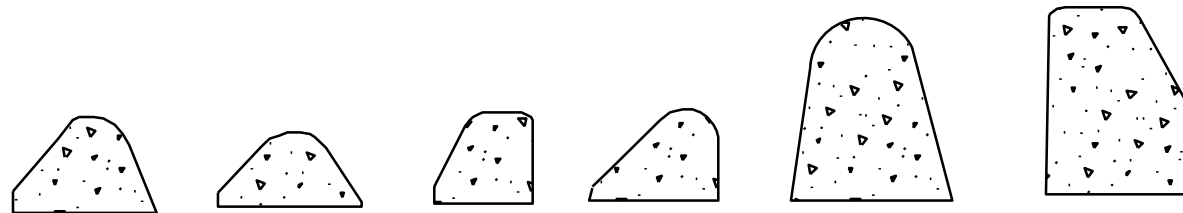
TYPE M-15



TYPE M-16




TYPICAL CURB DESIGNS  
TO BE PLACED WITH AUTOMATIC POWER CURBS



SKETCHES SHOWN ON THIS SHEET SHOW THE MORE COMMON CURB AND CURB AND GUTTER DESIGNS IN USE AND INDICATE SIZE RANGE OF CURB AND GUTTER THAT MAY BE EXTRUDED WITH POWER CURBERS. SOME VARIATION IN SIZE AND SHAPE OF CURB AND CURB AND GUTTER WILL BE ALLOWED IN ORDER TO ACCOMMODATE DIFFERENT MAKES OF POWER CURBERS.

--SPECIFICATIONS--  
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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<b>REVISIONS</b> 1. Added to CADD on 09-08-00 by J.F.T. 2. Deleted "MIX SPECIFICATIONS (FOR CONCRETE CURBS)" on 07-17-13 by J.F.T.		
 <b>ALABAMA DEPARTMENT OF TRANSPORTATION</b> 1409 COLISEUM BOULEVARD MONTGOMERY, AL 36130-3050		
DESIGN BUREAU SPECIAL DRAWING DETAILS OF CURB AND GUTTER MACHINE MOLDS (THIS SHEET FOR USE IN SELECTING MACHINE MADE CONCRETE CURB OR CONCRETE CURB AND GUTTER)		
Bureau Std Engr: <u>L.V.S.</u> DRAWN BY: _____ DATE DRAWN: <u>10-23-70</u>	SPECIAL DRAWING NO PC (ACG-71)	INDEX NO 730

NOT TO SCALE

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

DETAIL CALL-OUTS: 14

1.

RAMPS

15'-0" MAX LENGTH

8.3% (12:1) MAX RUNNING SLOPE

5.0% (20:1) MIN RUNNING SLOPE

2.0% (50:1) MAX CROSS SLOPE
2.

FLARES

10.0% (10:1) MAX RUNNING SLOPE
3.

LANDING (TURNING SPACE)

2.0% (50:1) MAX RUNNING SLOPE

2.0% (50:1) MAX CROSS SLOPE
4.

BLENDED TRANSITIONS

5.0% (20:1) MAX RUNNING SLOPE
5.

SIDEWALKS

5.0% (20:1) MAX RUNNING SLOPE 9

2.0% (50:1) MAX CROSS SLOPE
6.

DRIVEWAYS

8.0% (12.5:1) MAX CHANGE IN GRADE BETWEEN ROAD SURFACES AND DRIVEWAY

8.0% (12.5:1) MAX CHANGE IN GRADE BETWEEN DRIVEWAY AND SIDEWALK

GENERAL NOTES FOR CURB RAMPS AND SIDEWALKS:

1. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK-OF-SIDEWALK SUCH AS, A BUILDING ADJACENT TO THE SIDEWALK, THE TURNING SPACE SHALL BE 4'-0" MIN BY 5'-0" MIN. THE 5'-0" DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
2. SIDEWALK, RAMP, AND CURB RAMP SURFACES SHALL BE NON-SKID PREPARED BY BRUSHING.
3. 2% (50:1) OR LESS IS CONSIDERED FLAT.
4.

INSTALL EXPANSION JOINT MATERIAL WHERE NEW CONSTRUCTION MEETS EXISTING SIDEWALKS, CURBS, GUTTERS, OR FOUNDATIONS. THE CONSTRUCTION JOINT MATERIAL SHALL MEET THE REQUIREMENT OF SECTION 832.01 AND SEALED IN ACCORDANCE WITH SECTION 832.02.
5. THE SUBGRADE SHALL BE FREE OF ALL ROOTS AND OTHER UNDESIRABLE MATERAILS WITH THE ROOTS CUT BACK ONE (1) FOOT FROM FACE OF SIDEWALK.
6. SIDEWALKS, RAMPS, LANDINGS, AND CURB RAMPS SHALL BE FOUR (4) INCHES THICK UNLESS NOTED OTHERWISE.
7. RAMP AND CURB RAMP GRADE(SLOPE IN THE DIRECTION OF PEDESTRIAN TRAVEL) SHALL NOT EXCEED 8.3% (12:1).
8.

GUTTER COUNTER SLOPE SHALL NOT EXCEED 5.0% (20:1).
9.

WITHIN THE STREET OR HIGHWAY RIGHT-OF-WAY, THE GRADE OF THE PEDESTRIAN ACCESS ROUTES SHALL NOT EXCEED THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT STREET OR HIGHWAY. WHERE PEDESTRIAN ACCESS ROUTES ARE NOT CONTAINED WITHIN A STREET OR HIGHWAY RIGHT-OF-WAY, THEY SHALL HAVE A MAXIMUM SLOPE OF 20:1 (5.0%).
10.

PROVIDE A TWO (2) FEET MINIMUM TRANSITION STRIP IF ALGEBRAIC DIFFERENCES BETWEEN ROADWAY SLOPE AND CURB RAMP SLOPE ARE GREATER THAN 9:1 (11.0%). TRANSITION STRIP SHALL NOT EXCEED 2.0% (50:1).
11.

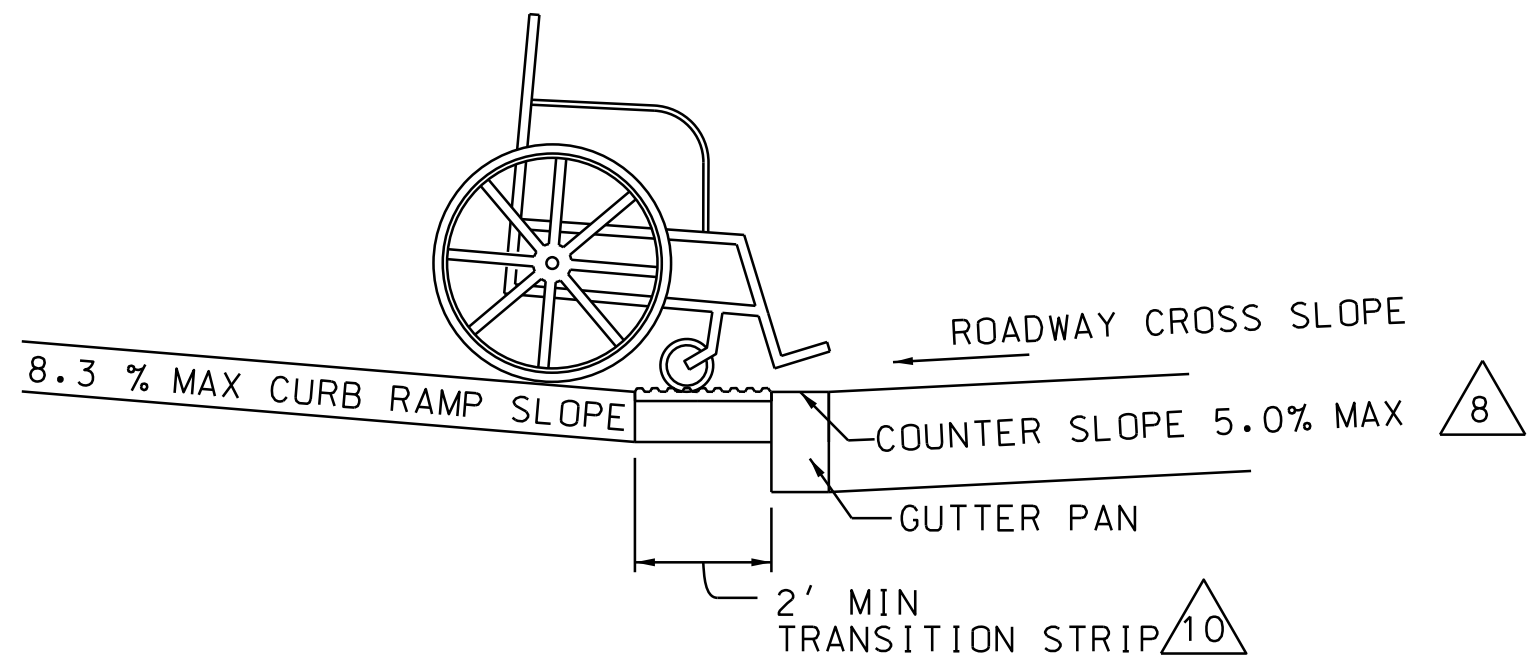
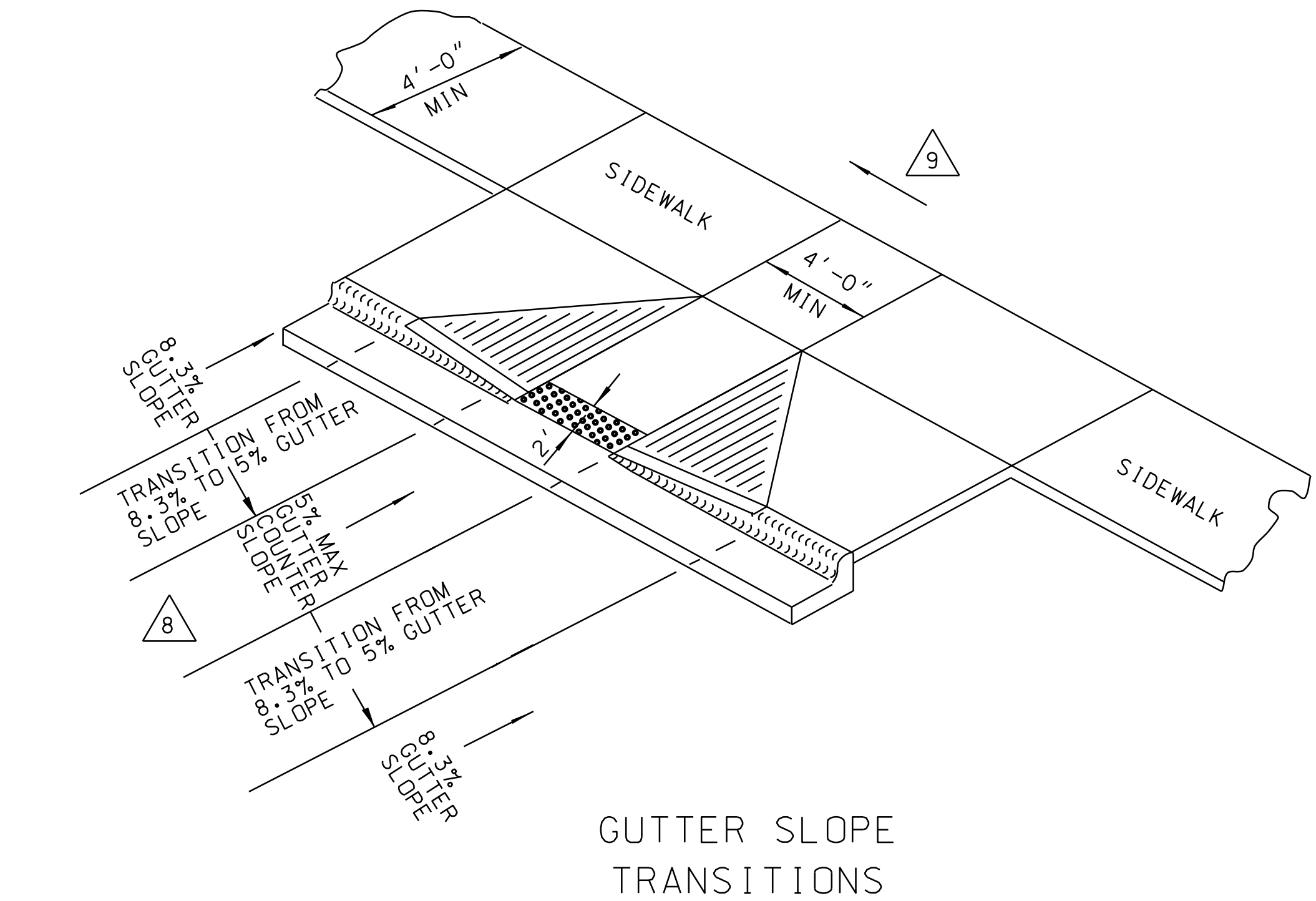
DETECTABLE WARNING SURFACES MUST BE TWO (2) FEET IN LENGTH IN THE DIRECTION OF PEDESTRIAN TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP. THE DETECTABLE WARNING MATERIAL AND MANUFACTURER SHALL BE FROM THE ALABAMA DEPARTMENT OF TRANSPORTATION LIST OF QUALIFIED MATERIAL, SOURCES, AND DEVICES. THE COLOR SHALL BE BRICK RED OR A COLOR APPROVED BY THE ENGINEER THAT SHALL CONTRAST THE SURROUNDING SURFACES LIGHT-ON-DARK OR DARK-ON-LIGHT.
12.

VERTICAL SURFACE DISCONTINUITIES SHALL BE 0.50 INCH MAXIMUM. VERTICAL SURFACE DISCONTINUITIES BETWEEN 0.25 AND 0.5. INCH SHALL BE BEVELED WITH A SLOPE NOT GREATER THAN 2:1 (50.0%).
13.

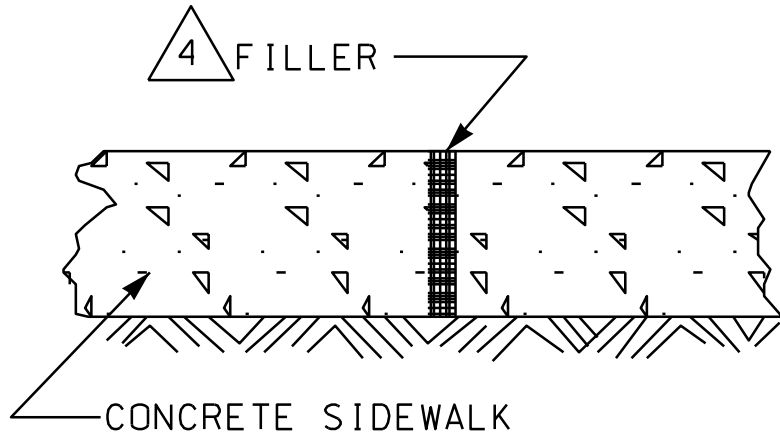
DOMES SHALL BE PARALLEL TO THE RAMP SO THAT THE WHEELCHAIR WHEELS CAN TRAVEL BETWEEN THE DOMES.
14.

THESE MAXIMUM SLOPES SHALL NOT BE EXCEEDED.
15.

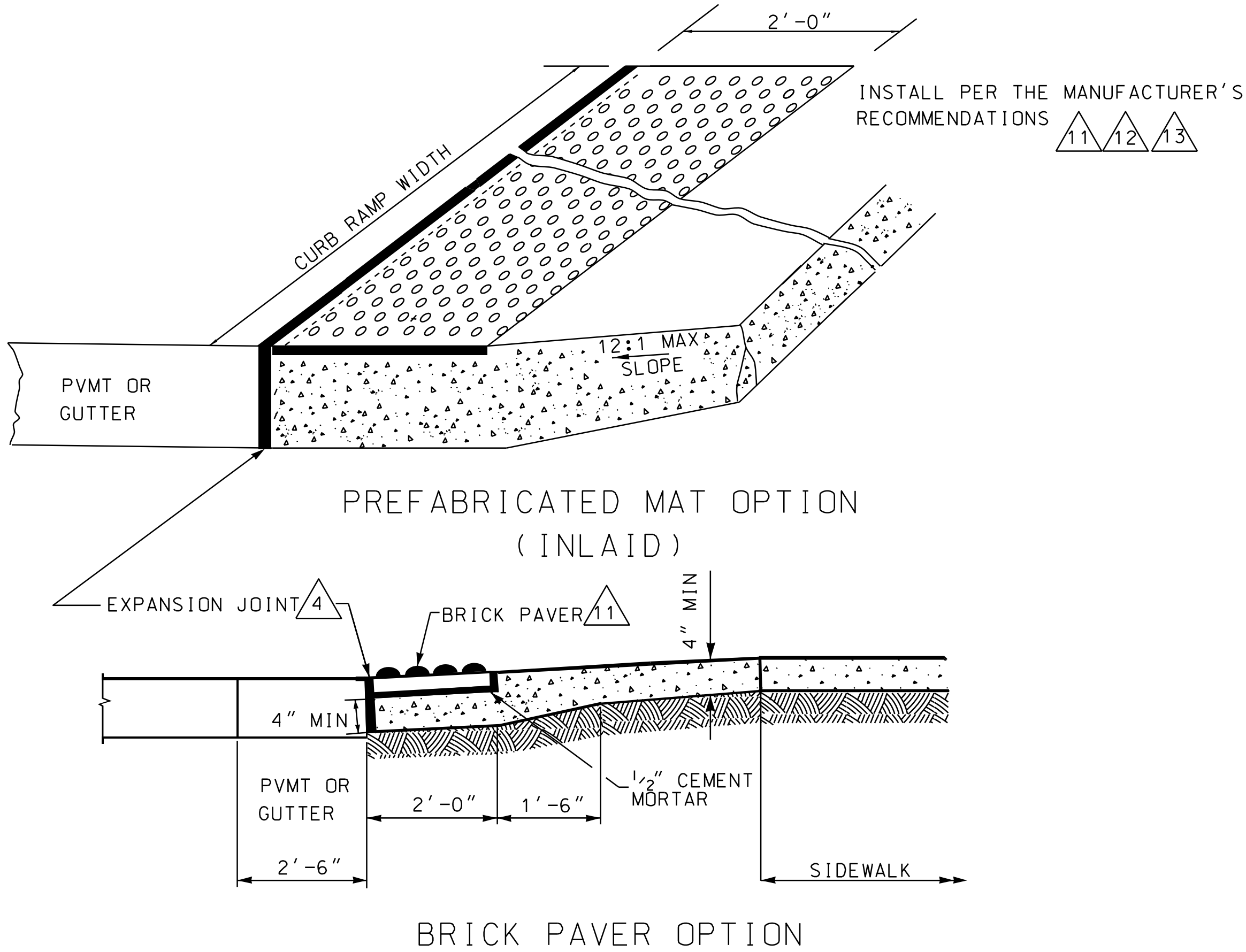
TO PREVENT STANDING WATER AT THE BASE OF CURB RAMPS, LOCATE STORM DRAIN INLETS UPSTREAM.



CHANGE OF GRADE  
LIMITATIONS



EXPANSION JOINT  
DETAILS



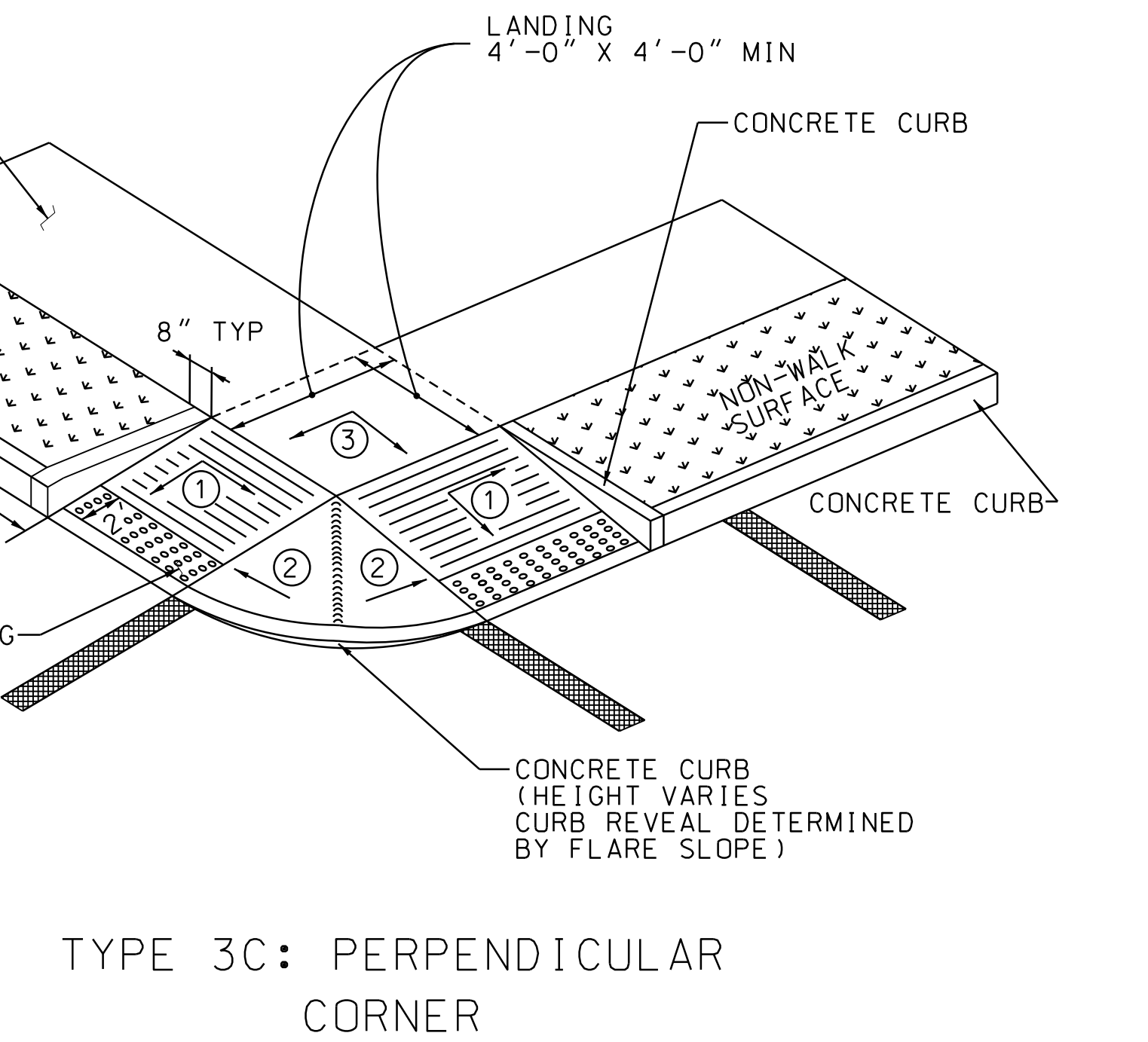
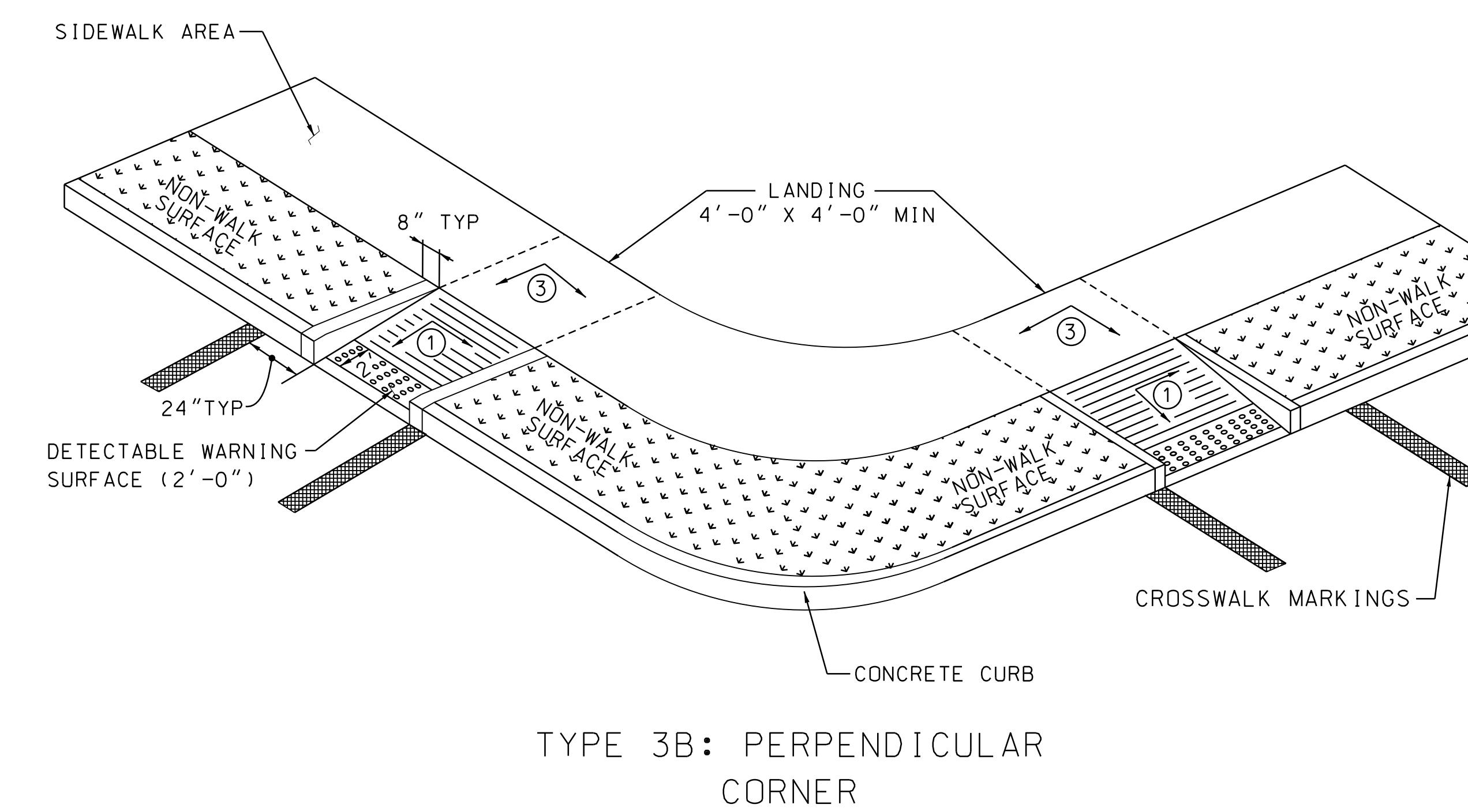
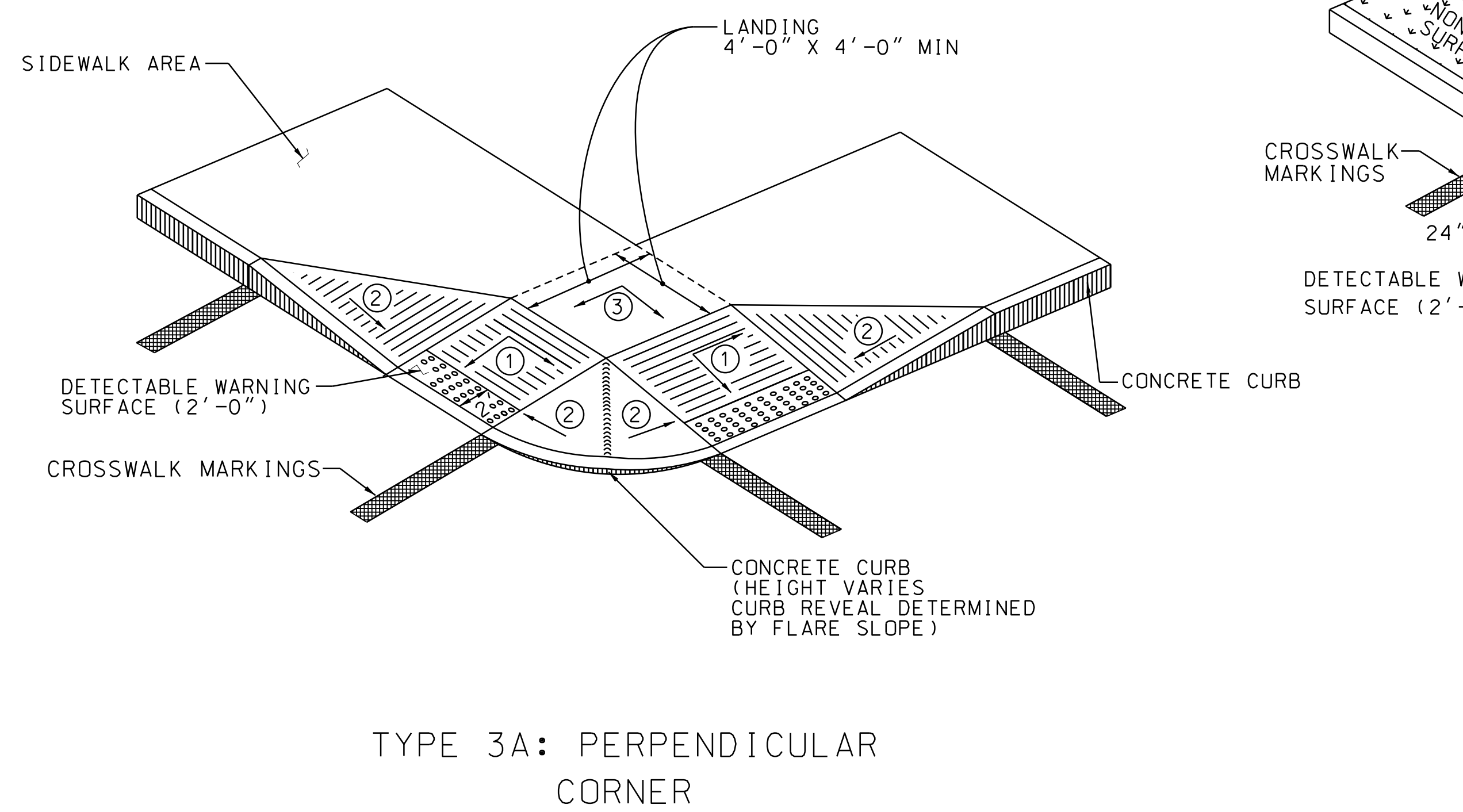
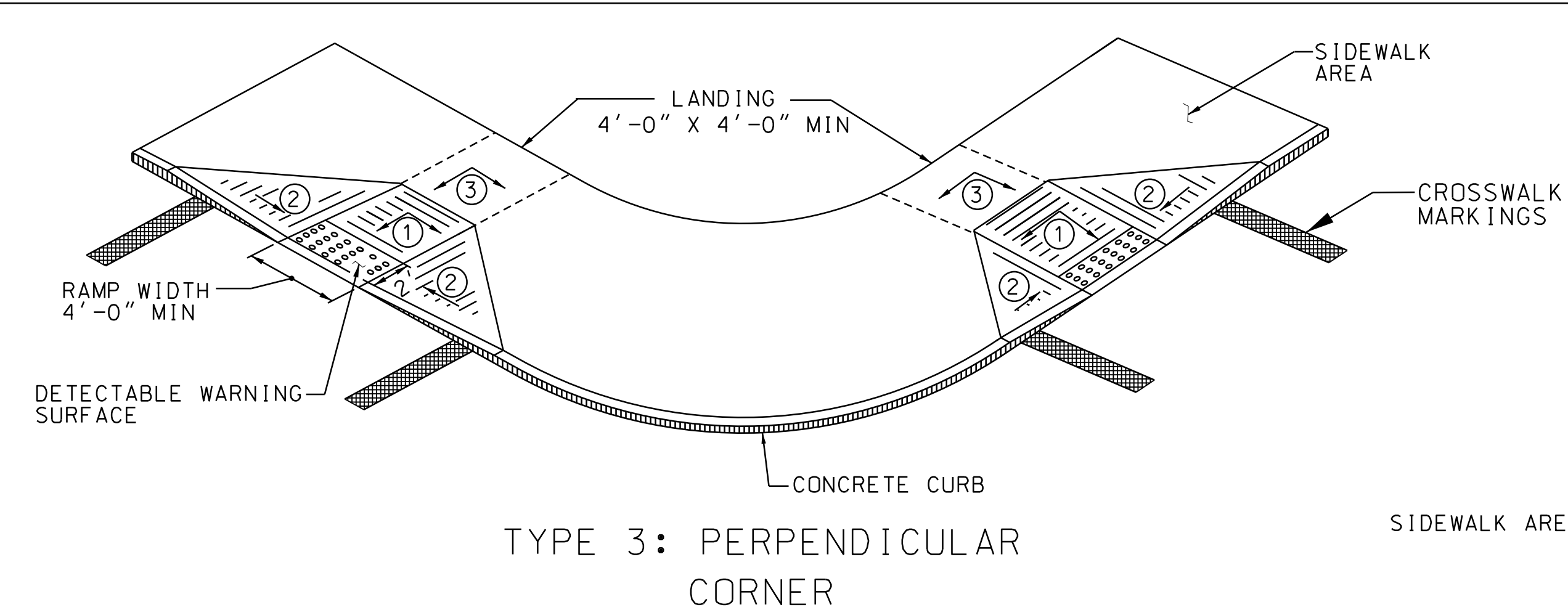
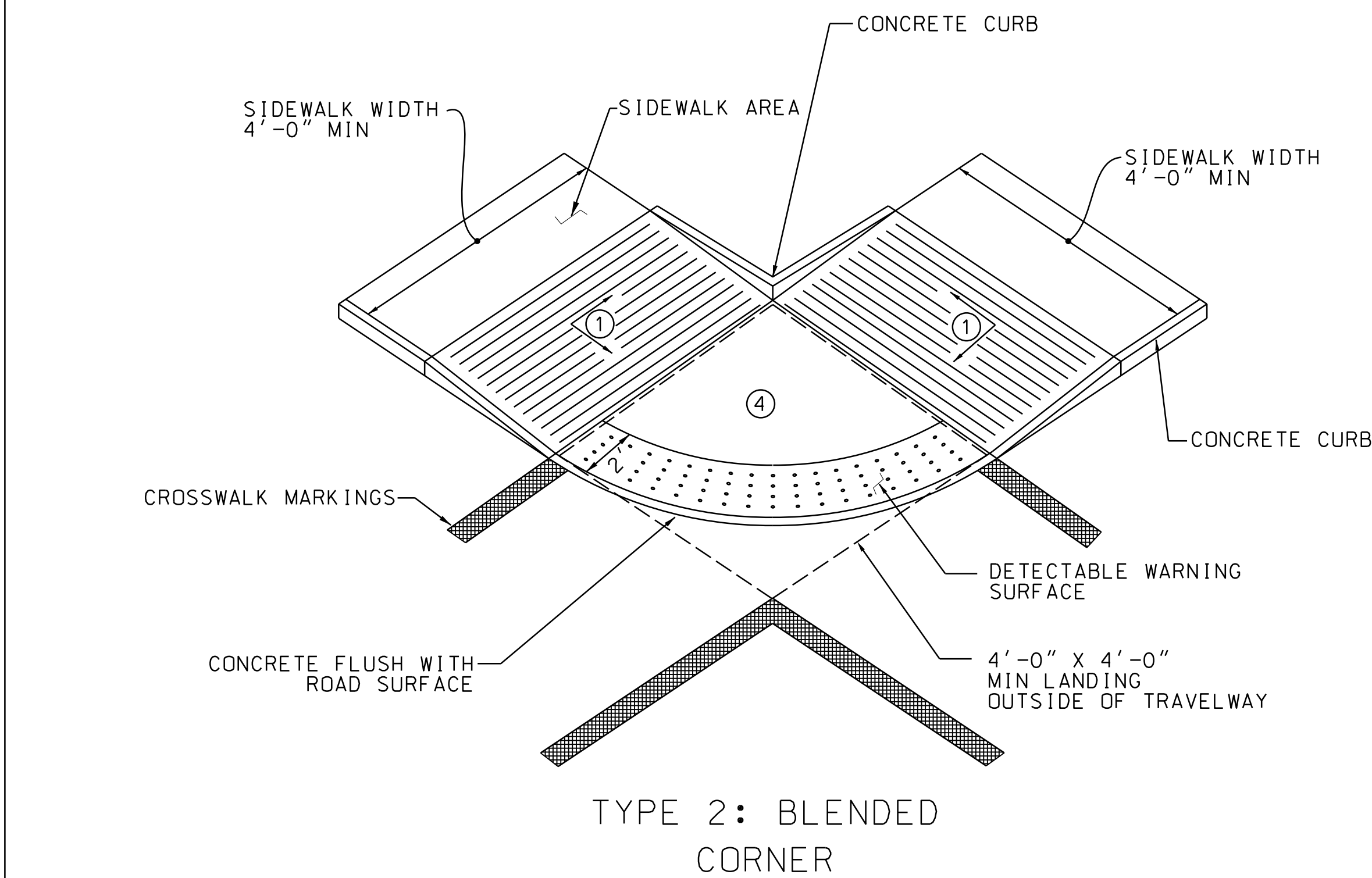
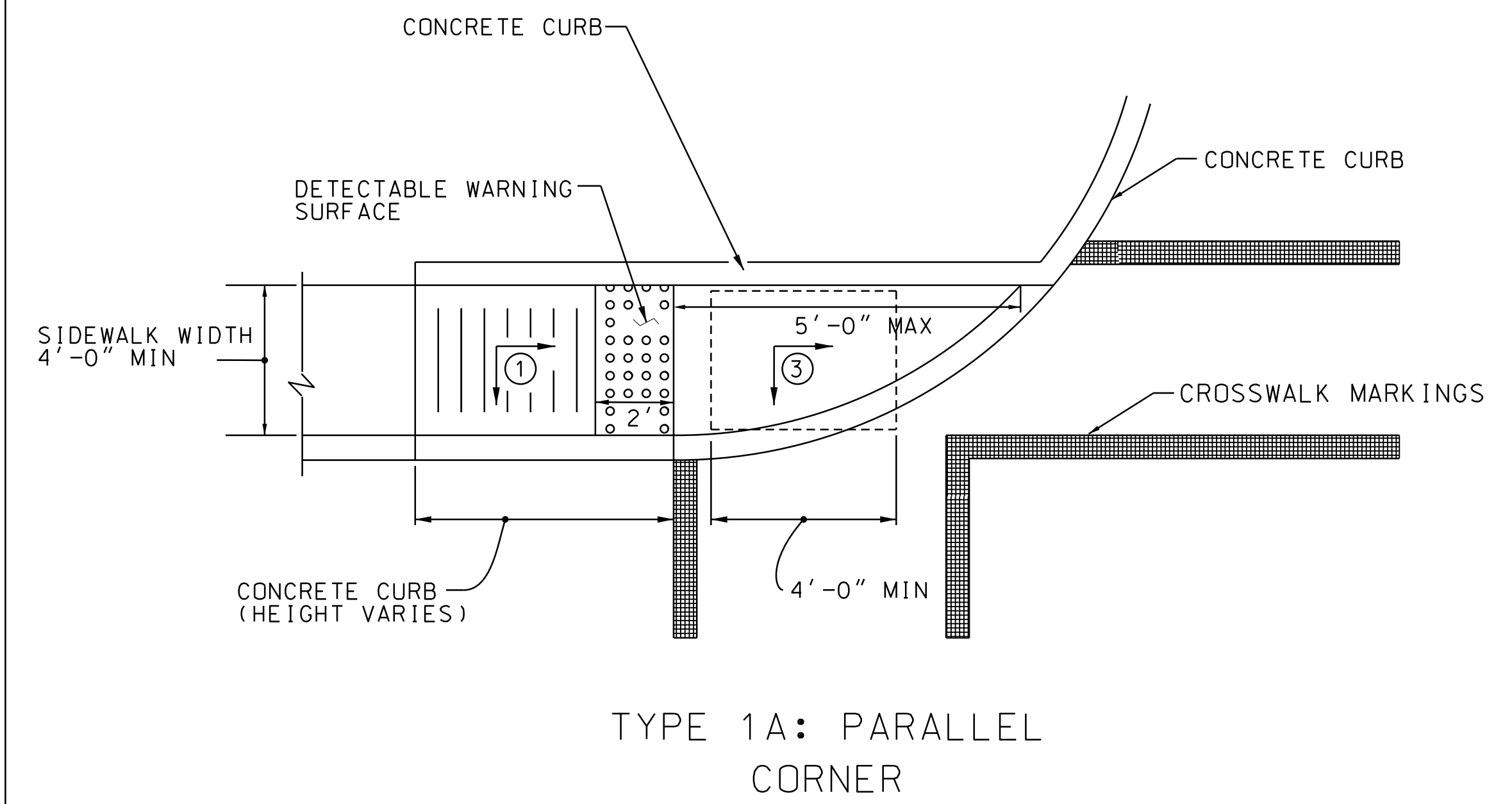
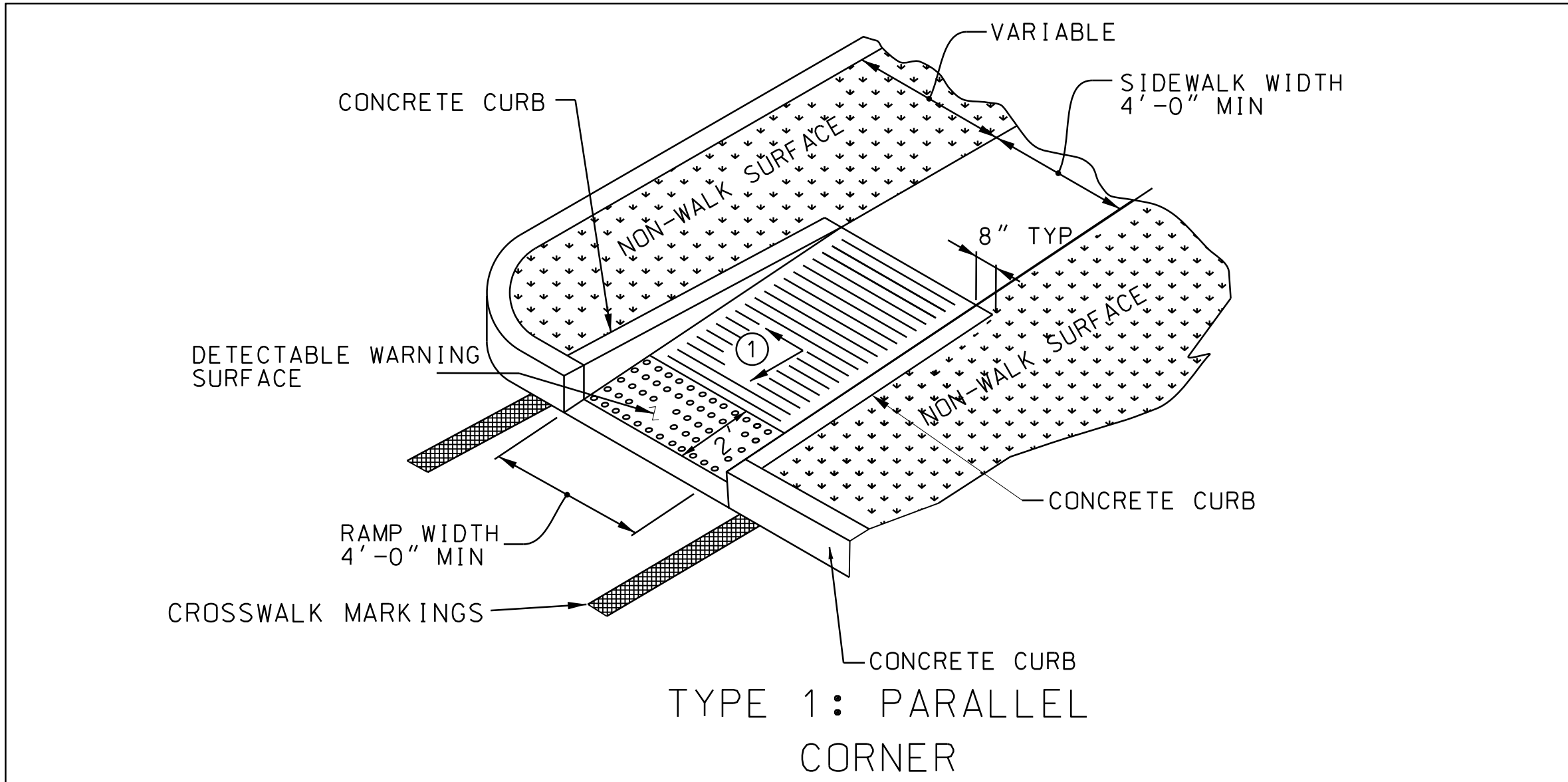
--SPECIFICATIONS--  
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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REVISIONS	<div><div><div><div></div><div></div></div><div><div></div><div></div></div></div><div><div>ALABAMA DEPARTMENT OF TRANSPORTATION</div><div>1409 COLISEUM BOULEVARD MONTGOMERY, AL 36130-3050</div></div></div>	
	DESIGN BUREAU SPECIAL DRAWING CURB RAMP DETAIL CALLOUTS, GENERAL NOTES FOR CURB RAMPS AND SIDEWALKS, AND DETAILS	
DRAWN BY: _____ DATE DRAWN: _____	SPECIAL DRAWING NO. SW-618 (SHEET 1 OF 4)	INDEX NO. 734


NOT TO SCALE



REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

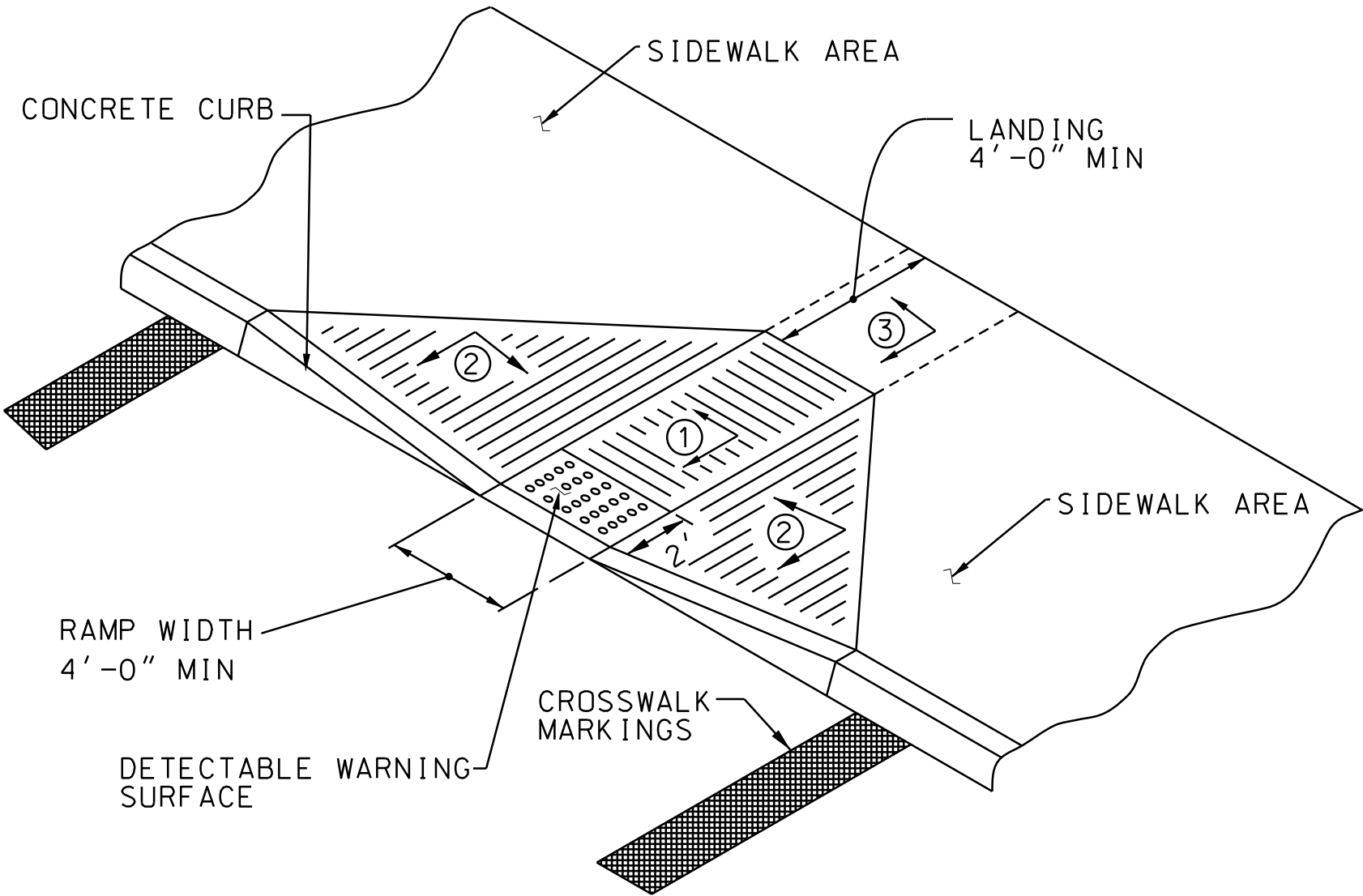


•SEE SHEET 1 OF 4 FOR DETAIL CALL OUTS ①, ②, ③, ④.  
•SEE SHEET 1 OF 4 FOR GENERAL NOTES FOR CURB RAMPS AND SIDEWALKS.

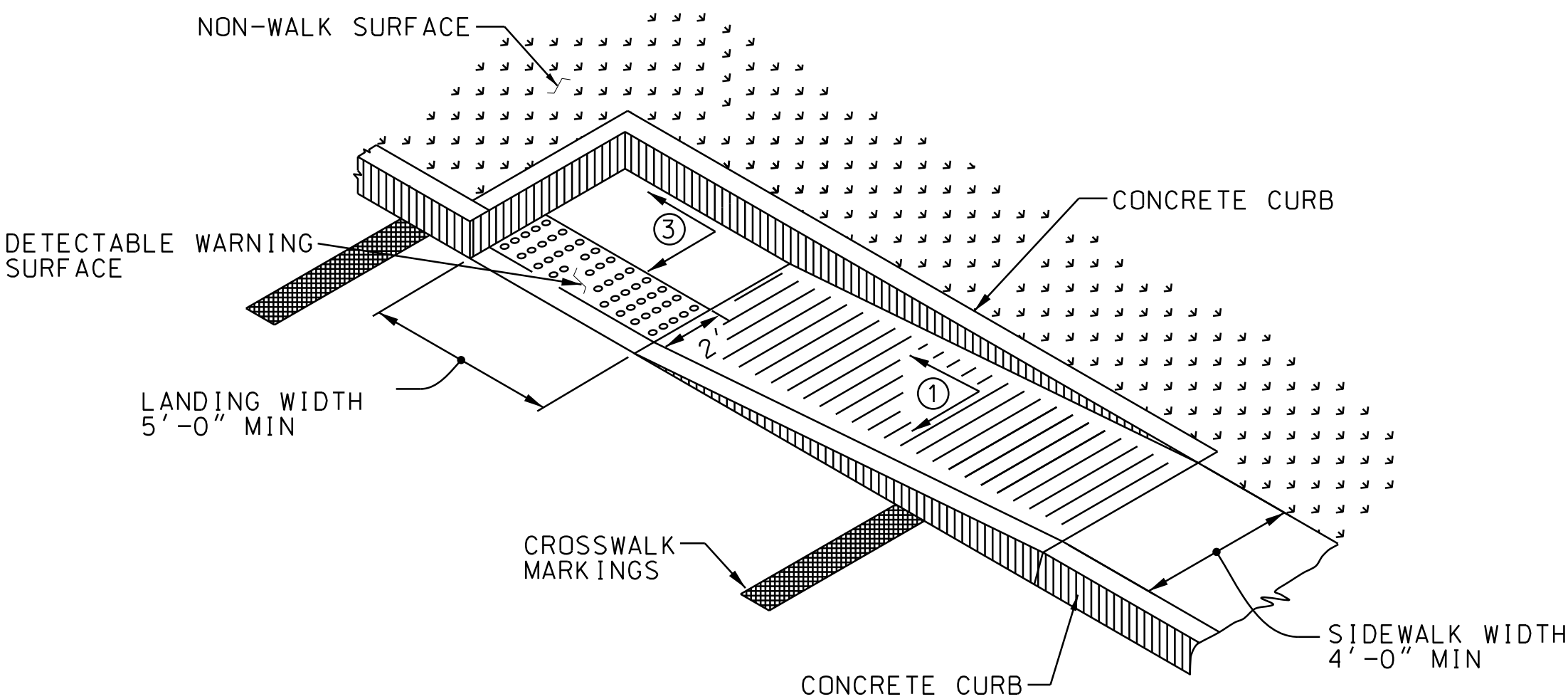
--SPECIFICATIONS-- CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION		
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REVISIONS	 <b>ALABAMA DEPARTMENT OF TRANSPORTATION</b> 1409 COLISEUM BOULEVARD MONTGOMERY, AL 36130-3050	
DESIGN BUREAU SPECIAL DRAWING  CORNER CURB RAMPS		
DRAWN BY: _____ DATE DRAWN: _____	SPECIAL DRAWING NO. SW-618 (SHEET 2 OF 4)	INDEX NO. 735

NOT TO SCALE

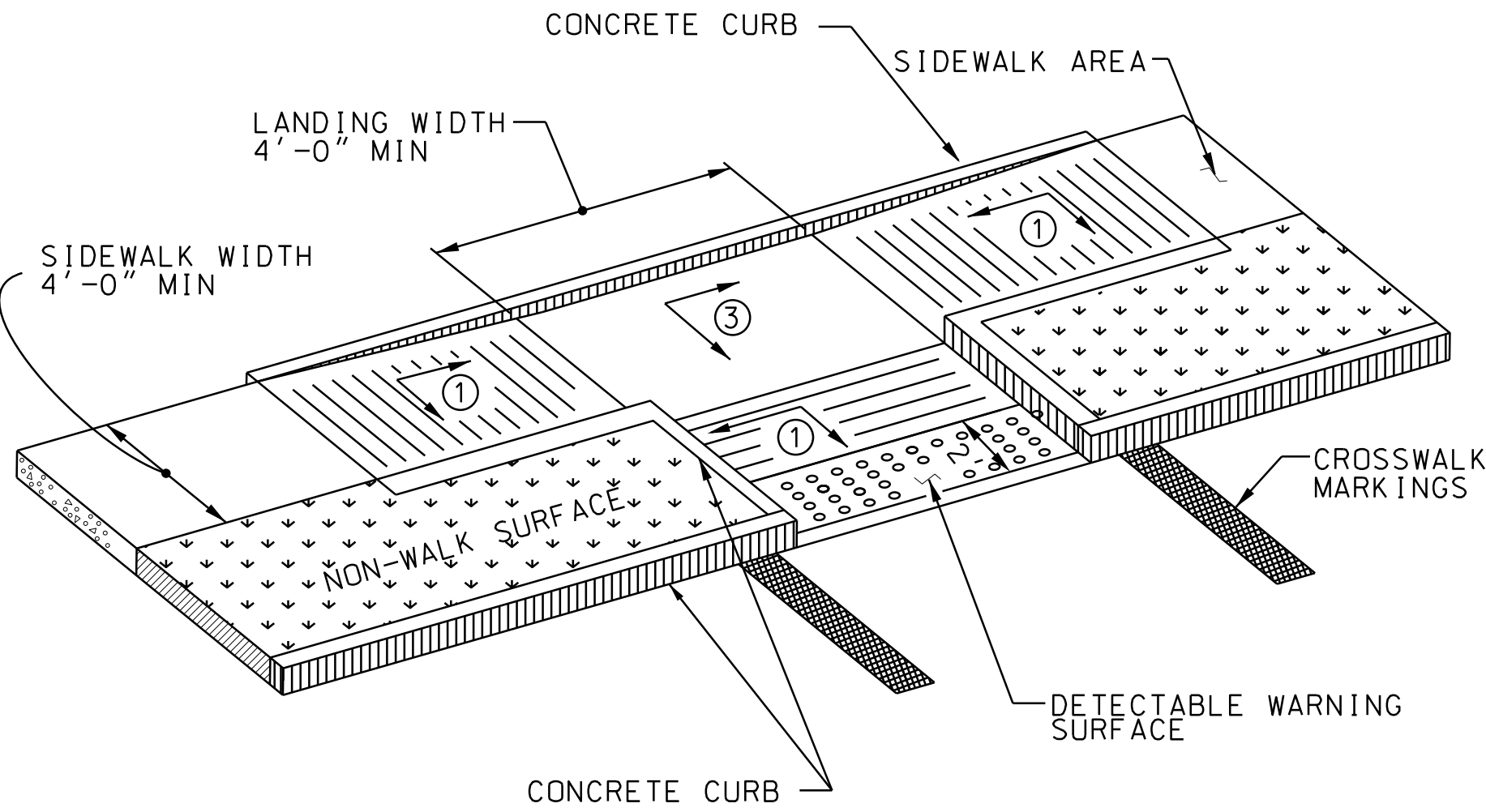
REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO



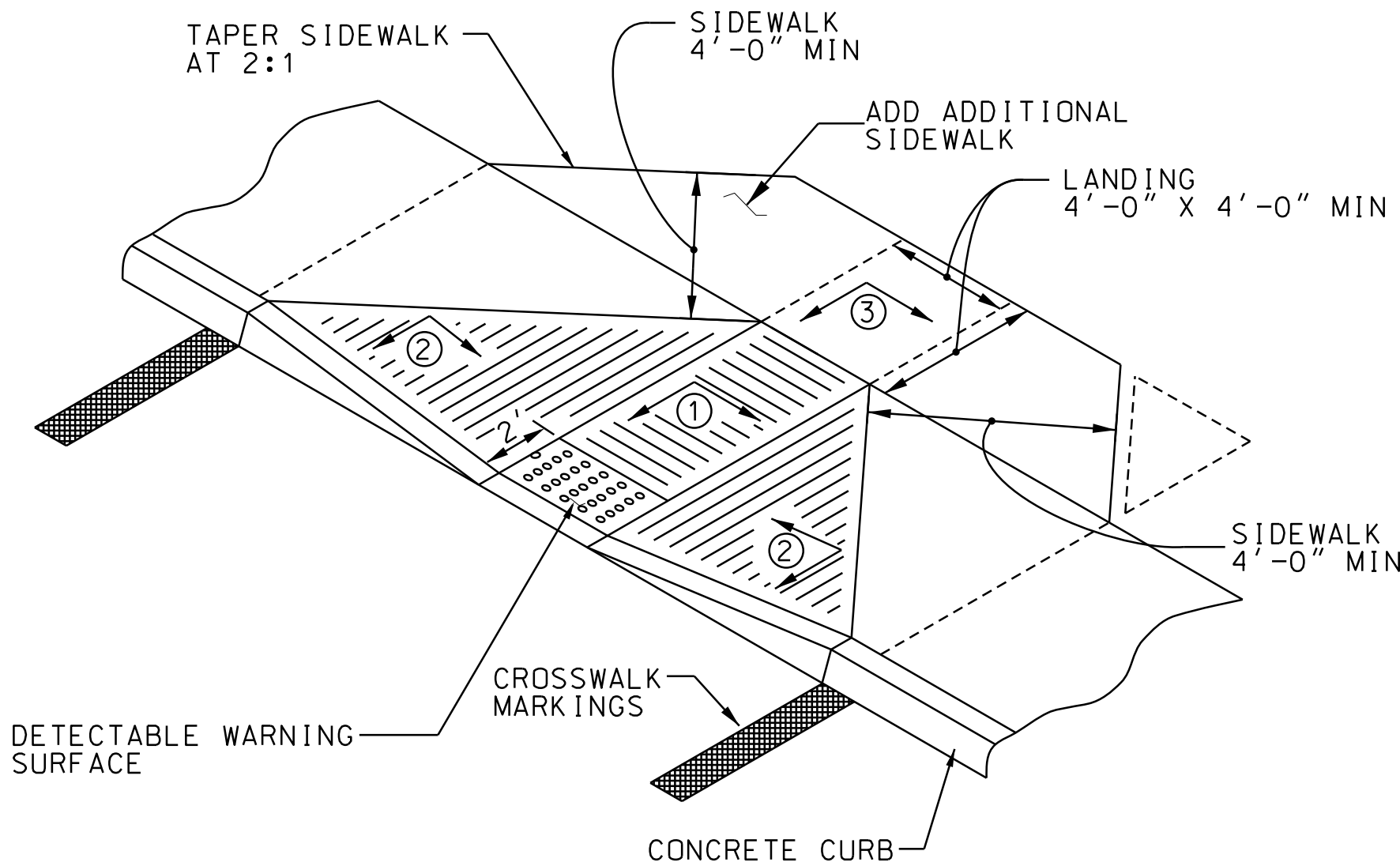
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MIDBLOCK



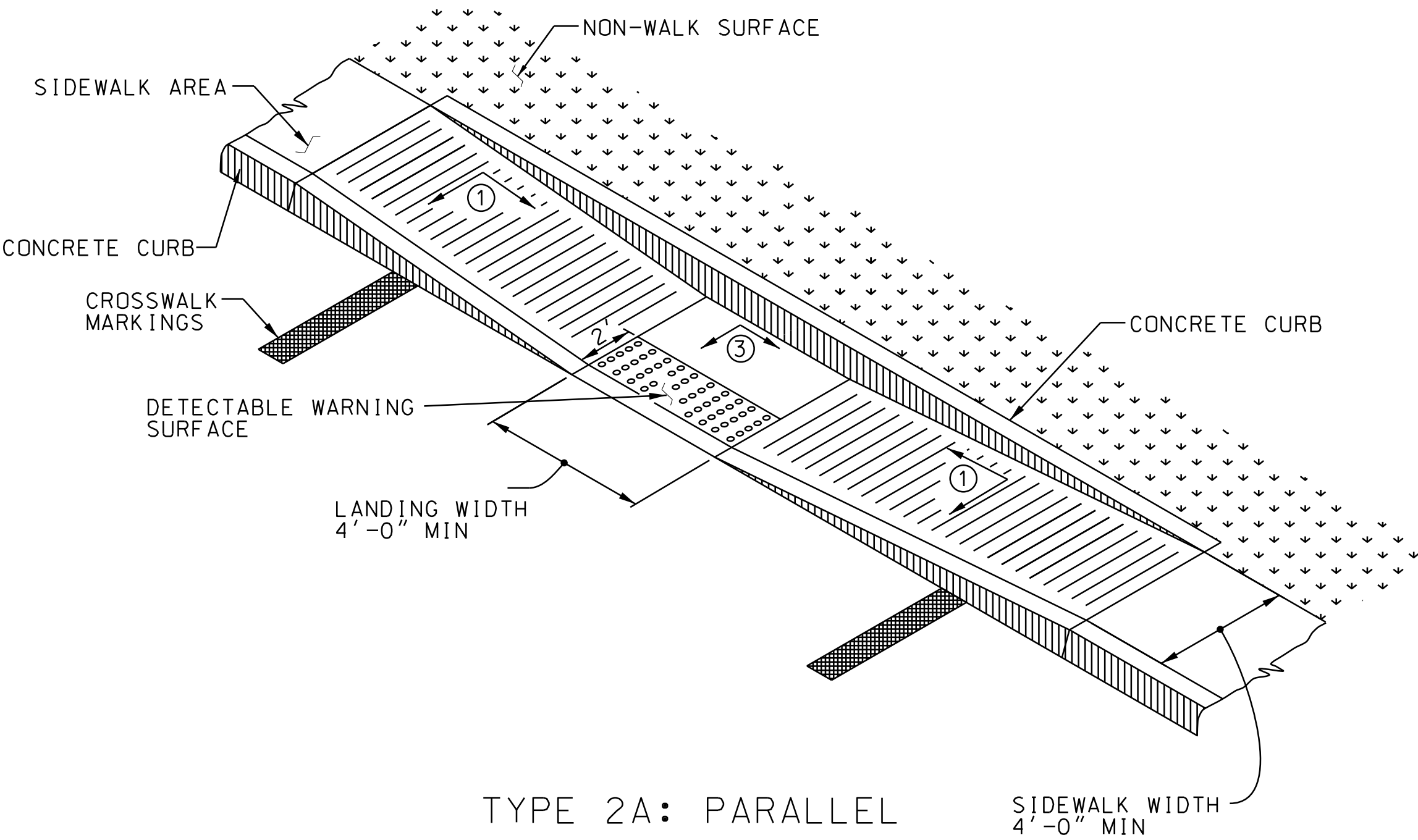
TYPE 2: PARALLEL  
MIDBLOCK



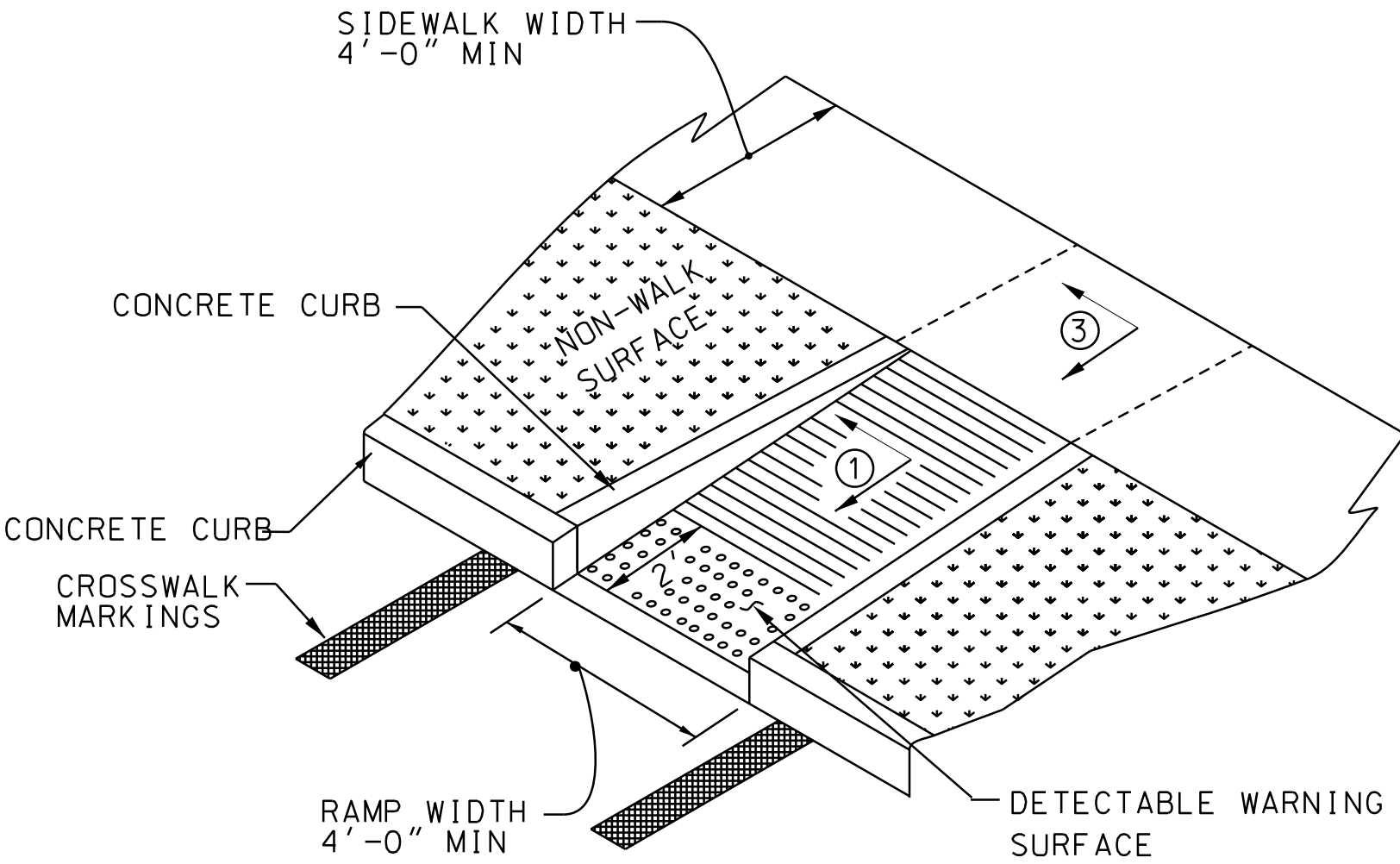
TYPE 3: COMBINATION  
MIDBLOCK



TYPE 1A: PERPENDICULAR  
MIDBLOCK




TYPE 2A: PARALLEL  
MIDBLOCK



TYPE 4: PERPENDICULAR  
MIDBLOCK

•SEE SHEET 1 OF 4 FOR DETAIL CALL OUTS ①, ②, ③.  
•SEE SHEET 1 OF 4 FOR GENERAL NOTES FOR CURB  
RAMPs AND SIDEWALKS.

--SPECIFICATIONS--  
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

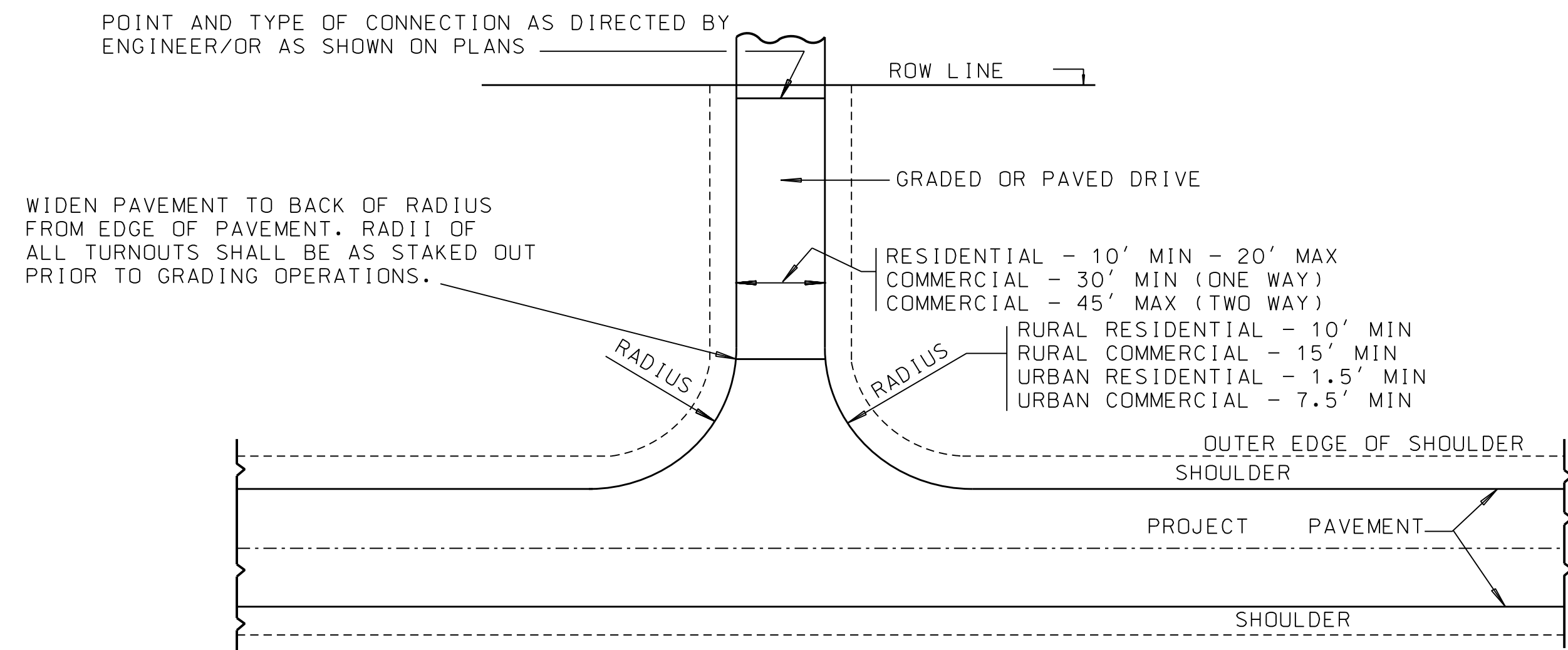
THIS DRAWING REPRESENTS DESIGNS PREPARED FOR USE BY THE ALABAMA DEPARTMENT OF TRANSPORTATION AND IS NOT TO BE COPIED, REPRODUCED, ALTERED, OR USED BY ANYONE, OR ANY ORGANIZATION, WITHOUT THE EXPRESSED WRITTEN CONSENT OF THE ALABAMA DEPARTMENT OF TRANSPORTATION REPRESENTATIVE AUTHORIZED TO APPROVE THIS USE. ANYONE MAKING UNAUTHORIZED USE OF THIS DRAWING MAY BE PROSECUTED TO THE FULLEST EXTENT OF THE LAW.		
REVISIONS	<div> <div>  <div> <b>ALABAMA DEPARTMENT OF TRANSPORTATION</b>  1409 COLISEUM BOULEVARD  MONTGOMERY, AL 36130-3050 </div> </div> <div>DESIGN BUREAU SPECIAL DRAWING</div> </div>	
	MIDBLOCK CURB RAMPS	
	DRAWN BY: _____ DATE DRAWN: _____	SPECIAL DRAWING NO. SW-618 (SHEET 3 OF 4)
		INDEX NO. 736

NOT TO SCALE

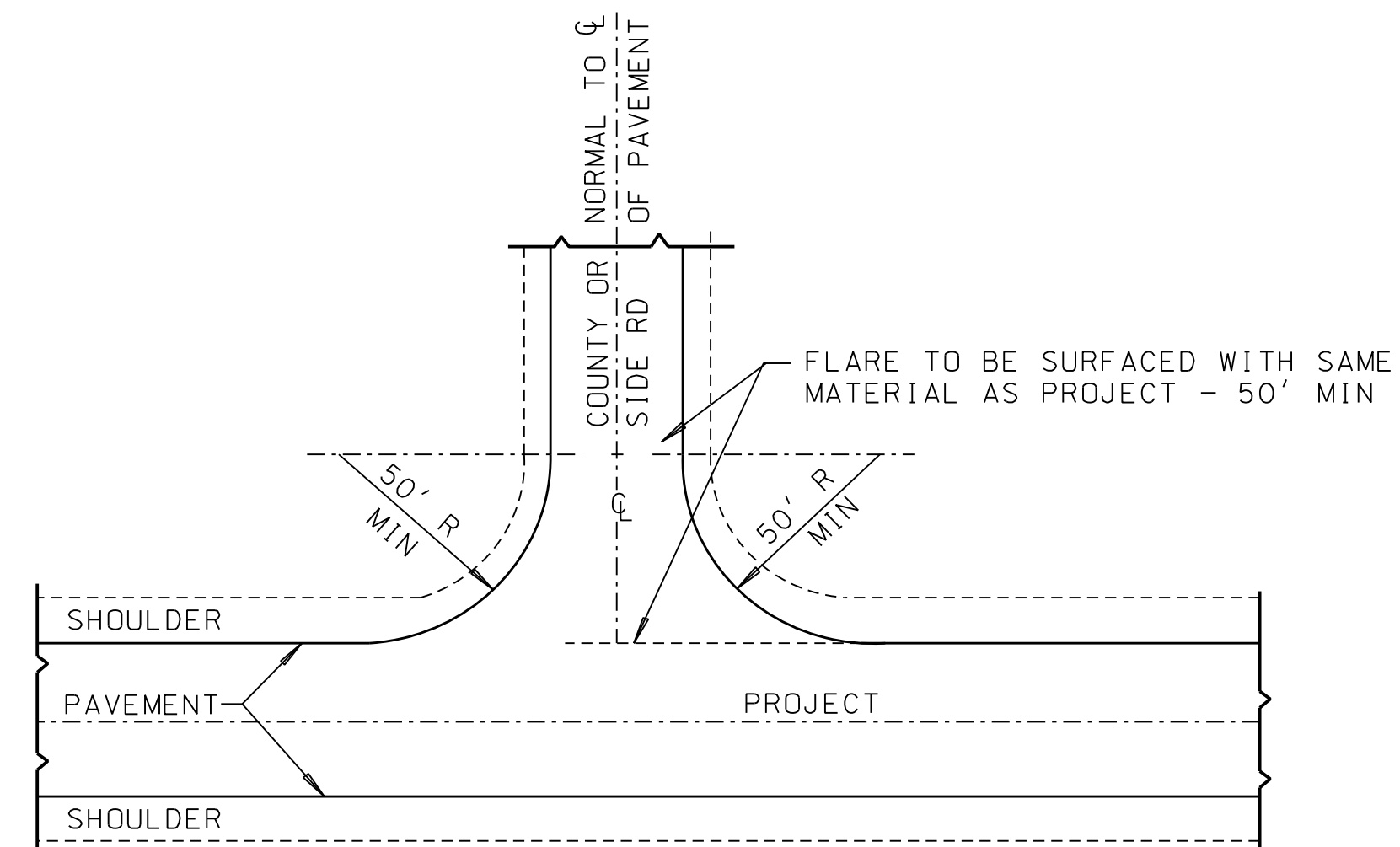




REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

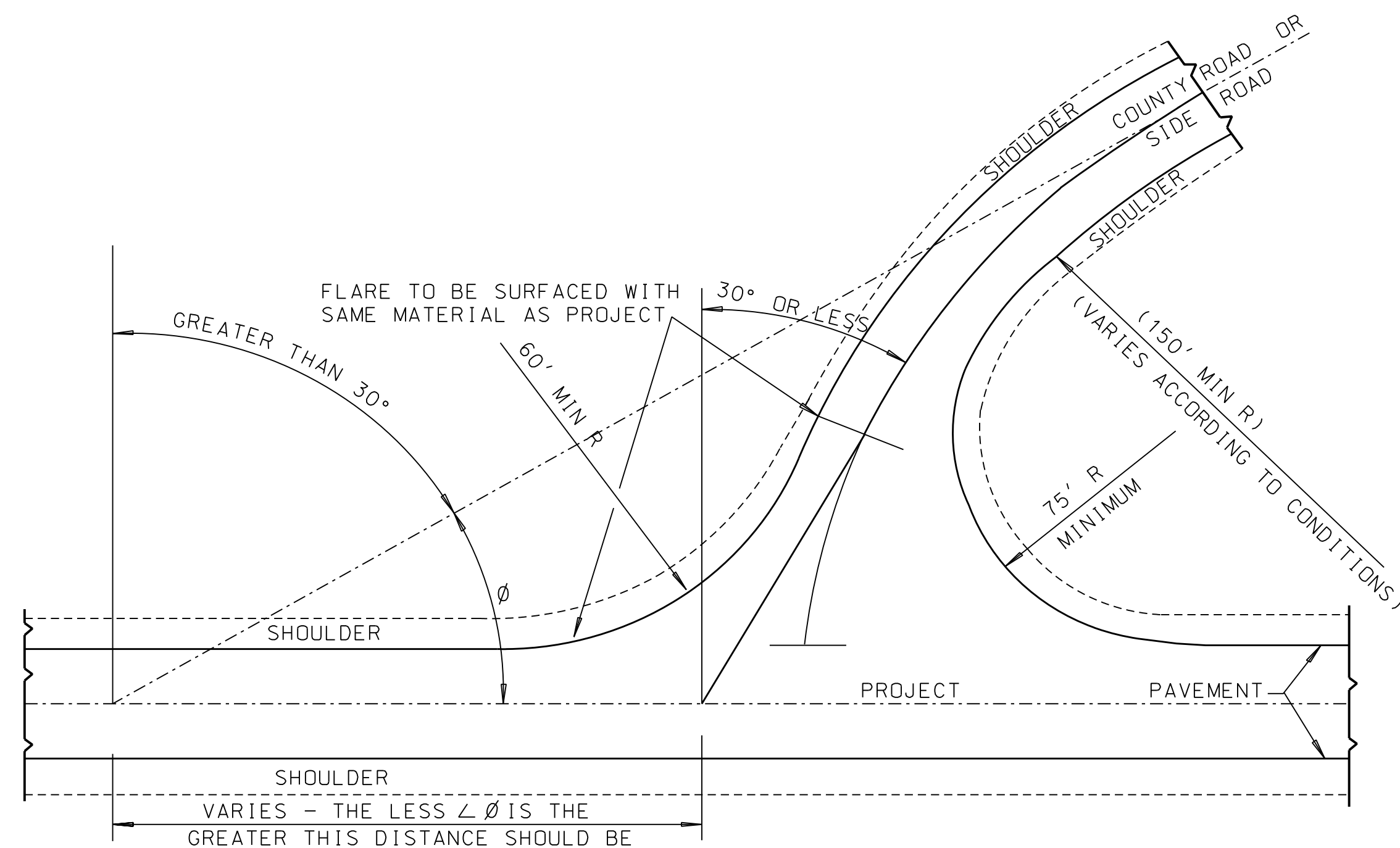


DETAIL OF TURNOUTS AT PRIVATE DRIVES



DETAIL OF INTERSECTION OF PROJECT WITH SIDEROAD (T-TYPE)

NOTE: USE THE PRIVATE DRIVE LAYOUT FOR PUBLIC ENTRANCES WITH APPROX 25 VEHICLES PER DAY. USE THE PUBLIC ROAD LAYOUT WITH 50' RADIUS FOR 25' TO 200 VEHICLES PER DAY. FOR PUBLIC ROAD ENTRANCES WITH MORE THAN 200 VEHICLES PER DAY RADIUS MAY BE INCREASED AS DIRECTED BY THE ENGINEER. WHERE RADIUS LARGER THAN 50' IS PROPOSED, THE LAYOUT OF THE INTERSECTION WILL BE SHOWN IN DETAIL ON THE PLANS.



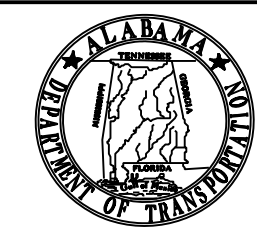
DETAIL OF INTERSECTION OF PROJECT WITH SIDEROAD WHEN BRANCH ANGLE IS GREATER THAN 30° FROM NORMAL

--SPECIFICATIONS--  
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

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REVISIONS

- Added to CADD to 08-23-00 by J.F.T.
- Noted 5' or 6' optional stripe width on 08-23-07 by W.W.A.



**ALABAMA DEPARTMENT  
OF TRANSPORTATION**  
1409 COLISEUM BOULEVARD  
MONTGOMERY, AL 36130-3050

DESIGN BUREAU SPECIAL DRAWING

**DETAILS OF INTERSECTIONS  
AND TURNOUTS**

Bureau Std Engr: L.V.S.  
DRAWN BY: \_\_\_\_\_ DATE DRAWN: 05-10-76

SPECIAL DRAWING NO  
**T0-107**

INDEX NO  
**738**

NOT TO SCALE